

BRIGHTON SPEEDWAY PARK OFFICIAL RULE BOOK

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Brighton Speedway Park encourages all drivers, pit crew and fans to support the sponsors of this great sport of ours. We all need to make a special effort and encourage others as well to support the sponsors of the race track for without them Brighton Speedway Park would not be here for us to enjoy.

BRIGHTON SPEEDWAY PARK OFFICIAL RULE BOOK

Introduction

Brighton Speedway urges you to carefully read and study all of these rules and become familiar with all aspects of racing at Brighton Speedway. By your participation in race events at Brighton Speedway, competitors will be deemed to have agreed to comply with these rules. No expressed or implied warranty of safety shall result from publications of, or compliance with these rules and/or regulations.

IF IT DOES NOT SAY THAT YOU CAN, CHANCES ARE YOU CANNOT, SO CHECK WITH TECH BEFORE YOU DO SOMETHING YOU ARE NOT SURE ABOUT.

Note: all 202⁵ Revisions are marked by a line in the left margin.

RULE BOOK DISCLAIMER

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules.

NO EXPRESS OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM PUBLICATIONS OF OR COMPLIANCE WITH THESE RULES AND/OR REGULATIONS. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official.

Brighton Speedway Officials shall be empowered to permit minor deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements. **NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM SUCH ALTERATION OF SPECIFICATIONS.**

Any interpretation or deviation of these rules is left to the discretion of the speedway officials. Their decision is final.

1.12 GENERAL RULES

1.1 The management has the right to reject the entry of any car or [person](#) at any time.

1.2 Definition of Behavior:

Anyone who, by word or deed, disrupts, impedes, jeopardizes or in any manner hinders the smooth and orderly presentation of the racing program or any other Speedway function, or anyone who commits, or causes to be committed, any action detrimental to stock car racing, will be considered to have abused the privileges and/or admission and may be subject to the revoking of those privileges and/or disqualification as deemed proper by management or designated official.

Drivers are responsible for the sportsmanlike conduct of anyone in their crew and penalties and [finefines](#) incurred by a crew member will affect the driver.

1.3 The point system will be in effect for the driver only.

1.4 A substitute driver may be used for two point's events only and drivers must notify officials prior to the start of the event. No registered driver may act as a substitute for another driver in their respective class and no driver may substitute in a class lower than they are currently registered in, [with the exception of Late Model and Canadian Modified drivers who may substitute back and forth. Substitute drivers will start scratch for the entire race program on regular points events. Non-point or show up point events, the substitute can draw and redraw for position as per normal procedures.](#)

1.5 No registered driver may purchase / borrow / swap their respective race car after either of the respective drivers involved has attempted to qualify, this includes time trials or timed hot laps as applicable.

1.6 The driver qualifying a car must race the car in the feature. All points and money are awarded to the driver, not the car.

[1.6.5 A car can qualify in only class per night.](#)

1.7 Any car going into the infield must re-enter on the same side of the track without interference to any other car in a safe and cautious manner. No passing is to be done other than on the racing track or that car will be penalized.

1.8 Only one vehicle will be allowed in the pit area with a stock car.

1.9 When driver meeting is being held in the pits, all cars must be shut off. **ALL DRIVERS MUST ATTEND MEETING.**

1.10 Anyone who, in the opinion of management, is under the influence of alcohol or drugs or has the possession of the same will be suspended for an indefinite period. The fore-mentioned substances are prohibited from speedway property.

1.11 No person in the pit area, other than the drivers racing, will be allowed west of the east bank of the creek after races commence unless permission is given by the official. This includes the bridges as well.

1.12 No driver will enter into any argumentative discussions with anyone. **NO FIGHTS OR ABUSIVE ARGUMENTS ALLOWED.**

1.13 Participation in a scheduled point's event will be defined as **paying entry fee, registering with the pit director, having an operable legal race car in the track pit area and attempting to qualify**. These criteria apply to any car or driver participating in any awards on that given night.

1.14 One driver may race two cars on the same night under the conditions he/she must make every attempt to switch from car to car in the fastest time permissible and understand the Speedway or officials are not obliged to hold an individual race for the driver to switch cars. If a driver is not prepared start a race, he/she will go to the scratch position.

1.15 Car numbers are to be painted neatly on both sides of the car, and on the roof legible from the right side of the car, at least 2" wide and 18" high. Numbers must be painted in contrasting colour. Number to be limited to no more than two digits. Letters, fractions, plus or minus signs will be allowed for driver preference only but will not be part of the driver's official number and for appearance or personal preference only. Speedway Officials have final say if a number is prominent enough.

1.16 **ROOKIE CRITERIA** - Drivers anticipating participation for first time in a new class must start SCRATCH for a minimum 3 weeks or until the speedway officials feel the driver is able to compete at the appropriate level. If that driver qualifies for the redraw through the heats, they will start in the last redraw position.

1.17 Any car that registers for racing on any given race meet **must look presentable and always meet safety and mechanical requirements**. It is the driver's responsibility to adhere to these standards or face appropriate penalties at the discretion of the officials.

1.18 Front/top 6" of the cars roof/visor must be left blank for each individual class sponsor, including Late Models.

2.0 CAR AND DRIVER REGISTRATION

2.1 All numbers must be registered before **1st of March** in order to retain the number for the current year. There will be a **\$30.00** driver registration fee to be paid online. Every driver entering a race event must fill out an official Brighton Speedway registration form (exceptions to alternate driver).

2.2 In 2025 Brighton Speedway memberships will not be required in order to be eligible for points fund money at the end of the season.

2.3 Drivers choosing to take part in the Brighton Speedway Late Model Point Fund must meet a minimum of four criteria to be eligible; compete in a minimum of 10 race events, finish in the top 15 in weekly points, attend the annual awards banquet and run sponsor decals.

2.4 Anyone under the age of 18 is considered a minor and must have a minor release form signed by both their parents (and legal guardian) and have a copy of their birth certificate before they enter the pits. Anyone between the ages of 14 and 15 will be allowed to race in the Stinger, Comp 4, Mini Stock to race in a higher class driver must prove significant race experience (i.e. go-kart racing). They must also pre-qualify by contacting the Speedway and may be subject to approval by Speedway's insurance provider. Pit admission could be revoked at any time by management.

2.5 Registration and memberships are only valid for one season and may be revoked at any time.

2.6 Any car will be permitted to run at Brighton Speedway if they meet the safety/mechanical requirements. They will be subject to weight handicap and other restrictions the Speedway feels appropriate.

3.0 RACE PROCEDURES

3.1 One-way receivers will be mandatory for all classes for the speedway race director to communicate to the drivers at all times. No Two Way Communication of any kind with anyone other than a track official once you have entered the racing surface.

3.2 AMB Transponders are mandatory and required for scoring of all Mini Stocks, Comp 4, Thunder Stocks, Canadian Modified and Late Model events. Late Models and Canadian Modifieds to mount their Transponders on the rear face of the engine mid-plate. Comp 4, Mini Stocks and Thunder Stocks to put their transponders 18" behind the centreline of the rear end with a clear view to the race surface.

- 3.3 All Saturday night racing will start at **7:00 p.m.** All cars must be registered one hour before designated race time. Cars late to register will run scratch position for the entire night. Pit entrance will open at 4:30 p.m. All cars must pass tech inspection. Any car not inspected or not meeting necessary requirements will not be allowed to run - **NO EXCEPTIONS**
- 3.4 It is mandatory that all participating race cars assist in the packing of the track as required. Any race car not taking part will not be allowed to have hot laps and may run scratch for the entire evening at the official's discretion.
- 4:30 - 5:50
 - 5:50 – 6:00
 - 6:10 - 6:25
 - 6:55
 - 7:00
 - Driver Registration
 - Drivers Meeting
 - Hot Laps
 - National Anthem
 - Races Begin
- 3.5 Heat Line-ups - All Classes will draw at the pit shack every week in 2025 for qualifier positions.
- 3.6 Feature Line-ups- All Classes will redraw every week, top 4 from each heat if there are two heats and top three if there are three heats. All drivers that do not make the redraw from the heat will start heads up behind the redraw line up. The winner of the previous week's feature can start no better than the last redraw position for the feature. Note that for all divisions these procedures may be changed from time to time for special events.
- 3.7 All cars will be lined up in the pits. If a car is not in position when cars are going on to the track that car will run scratch.
- 3.8 Race lengths, each driver in each class will run one qualifier of 8 laps (6 laps for Stingers) in length and a feature. Feature lengths are as follows; Stingers – 12 laps, Comp 4 – 15 laps, Pro Stock – 20 laps, Late Models 30 laps and Canadian Modified 25 laps. **Note: these race lengths for both qualifiers and features may be adjusted if the Speedway Officials feels necessary due to car counts or nightly programs.**
- 3.9 Any feature event taking more than **25 minutes** in duration from the drop of the initial green flag, officials will start to count yellow flag laps. If a yellow extends into the white flag lap the race there will be one (1) attempt at a green white checker. If this attempt is unsuccessful the race director will drop the checker under yellow and deem the race official. It will be the race director's discretion whether the time limit will be increased for extra distance races.
- 3.10 In the event of rain, if all of the qualifying heats with points have been run, but no features, the race night is considered complete and final. If any features have been completed, the balance of the features will be made up at a later date. . If less than all of the qualifying heats with points have been run, the race meet will be cancelled. Registered drivers for that night only, will be admitted free the next race night.
- 3.11 Blocking of any sort will not be permitted. No mirrors or two-way radios allowed.
- 3.12 Cars that are responsible for a restart will return to the scratch position on the new line-ups. This includes the first lap of any race. During a restart any car that is not available for a line-ups when it is being formed will go to the scratch position. Classifying cars being involved in an accident will be at the discretion of track officials. If an accident occurs after the lead car has taken the checker flag any cars involved in the accident will assume scratch position.
- 3.13 Any car **spinning out** and lingering long enough to cause a yellow or a red flag or causing a yellow for a flat tire without being towed, will be restarted at the rear of the field and may, at the starter's discretion, be penalized one lap for delay of the race.
- 3.14 Any car leaving the track for repairs at any time will be run at the back in the heat. No repairs to be done on the track surface, or right edge of the track. No driver will use the tow truck for repairs on the track except to be pushed in the event their car has stalled. If convenient and time permitting the tow truck may set a car on jack stands if the crew is ready when the truck and car arrive to the pit.
- 3.15 Any driver involved in 2 cautions will be disqualified for that race at race official's discretion. The driver will receive last place points and money
- 3.16 Restart Procedures, all restarts will be done as double file restarts. Leader and only the leader may pick is line (inside or outside) all other cars must fall into their respective position. All restart line-ups will be done by the last lap scored.

3.17 During restarts all lapped cars will be put to the back of the of the lead lap cars. In feature events only during each caution flag the first car not on the lead lap will be given the "Lucky Dog" and get back one lap. If that one lap is enough to put them back on the lead lap, then they will be placed at the tail end of the lead lap cars before the rest of the lapped cars. Lucky dog will not apply to the driver that is deemed to have caused the caution.

3.18 Any competitor stopping on the race surface to dispute official's decision may automatically be disqualified from that race and may be suspended for the balance of the racing season. Any further suspension, or penalty or fine will be at the discretion of the officials.

3.19 No driver at any time may enter the officials tower without invitation. Any driver doing so is subject to complete disqualification for the night and potential further fines or suspension.

4.0 SAFETY PROCEDURES

4.1 **No driver may remove their safety equipment (including seat belt) or exit their car while it is on the racing surface.** Drivers must always stay in their cars even under caution and red flag conditions unless explicitly told to exit the car by the race director or an on-track official.

4.2 Car must first pass through safety inspection and receive approval sticker (see Inspection Sheets at back of book). No car will be allowed on the track for either practice or competition without the approval sticker but is subject to a further inspection as a result of an official's decision or a valid protest. The original inspection and resulting decisions shall be superseded by such a further inspection and resulting decisions. Any car deemed applicable may be subject to mid-season safety inspections.

4.3 Drivers in all classes including Stingers must wear at minimum SFI rated fireproof suit in good condition with no rips or tears unless otherwise specified. Late Models and Modifieds must wear at minimum a 2-layer SFI 3.2A/5. SFI rated Fireproof gloves are mandatory in all classes. Boots, shoes, underwear and head socks are also highly recommended. **FIRE HAPPENS FAST AND YOU NEED TO BE PREPARED, instead of spending all your money on horsepower please consider your personal safety**

4.4 All cars must have adequate hook and 1/4" chain, cable or solid accessible bar for towing purposes - front and back.

4.5 All Stinger, Mini Stock, Comp 4 and Thunder Stock cars must be equipped with window nets. Net size must be at least 16" x 18". Net must be installed so it is tight. Window net anchors must be attached to the roll bars, not body. Window net must be quick release type. Net must be permanently anchored at the bottom and release at the top. We highly recommend wide mesh nets. Window nets optional on Canadian Modifieds and Late Models provided a full containment seat is being used.

4.6 Aluminium High back racing seats are required for all classes with full containment seats recommended in all classes. Seats must be securely attached to the driver's cockpit cage and not the floor or any other part of the car. Stingers may use the original OEM manufactures mounting points to secure their seat structure.

4.7 Five-point harnesses, quick release approved seat belts that include minimum 3" in width lap belts, 2" in width shoulder and a crotch belt is required for all classes. All classes including Stingers must have belts that fall within the expiry date on the label. All belts must be in good condition with no fraying and no holes. Date stamp on the belts must always be legible throughout the season. Tech officials will have final say if condition is less than satisfactory. Belts must be secured by at least 7/16" diameter minimum grade 8 bolts attached to driver's cockpit cage and not the floor or any other part of the car. No camlock belts will be permitted. - Stinger Cars may use original OEM mounting points to mount the lap belts only.

4.8 Approved full-face helmet are compulsory and must be worn when the car is on the track for any reason. All Divisions must use helmets rated to SNELL SA2015 or higher. Snell 2015 is still allowed. 2025 will be out soon and that will phase out the 2015. New Helmets should be SNELL 2020 or newer.

4.9 **Steel screening with maximum 1" sq holes** and vertical support bars minimum 1/4" thick, no more than 6" apart or windshield or Lexan compulsory. For Stinger, Mini Stock, Comp 4 and Pro Stock the screening must cover the complete window opening. For Canadian Modifieds and Late Models the screening must cover the complete driver's compartment.

4.10 Drawings for sample cages can be found on page 36 of this rule book. Singers require a minimum of one main hoop behind driver's seat and one door bar on each side. Mini Stock, Comp 4 and Thunder Stock must have front and rear hoop with complete halo protecting the cockpit with a min of 4 driver's door bars and two passenger side bars. Cage roll bars must be of tubing not less than 1-1/2" outside diameter and have a minimum wall thickness of .120 or 1 3/4 of .095 wall thickness. All main roll cage members must be welded to frame or sub frame of the car. Canadian Modified cage requirements can be found in the UMP modified rules and the Late Model requirements can be found in the RUSH LM rules.

4.11 1/8" steel intrusion plates are highly recommended on the driver's door for all classes and are mandated for Late Model and Canadian Modified. Plates can be welded between existing bars or bolted on to the exterior of the driver's door bars as per the UMP rules for Canadian Modified and Rush Rules for Late Model.

4.12 All cars must be equipped with a serviceable type of sealed tube fire extinguisher, within arm's reach of the driver when his/her seat belt is fastened. Fire Extinguisher must be in quick release holder and easily removed in case of emergency. On board fire suppression systems are highly recommended in all classes but will be mandated in LM and Modifieds for the **2024** race season, **instead of spending all your money on horsepower consider your personal safety.**

4.13 All drive shafts must be painted white and must have safety hoop about 6" behind front u-joint.

4.14 All batteries must be securely mounted. Any battery mounted in drivers' compartment must be totally enclosed in a plastic container behind the driver's seat.

4.15 All cars must be able to shift forward and reverse while engine is running. All gears must be operable.

4.16 CM's, TS's, C4 and Mini Stock must have a KILL SWITCH installed within easy reach of the driver and/or track official. The switch must be identified by a yellow triangle not less than 4".

4.17 All cars must have hood pins on the hood and the trunk lids.

4.18 When weight is added to a car it must be bolted to the cage, frame or chassis in a safe manor, painted white with the car number painted on it! No ballast may be attached to a moving suspension. Loose or unsafe weight will be subject to a fine.

4.19 At minimum, foam helmet supports (neck guards) are required in all divisions. Head and Neck restraints are highly recommended for all classes and a 25lb weight break will be given to any driver in the Thunder Stock, Mini Stock, Comp 4 or Stinger divisions -that uses a Head and Neck restraint rated to SFI 38.1 rating.

4.20 All wheel covers must be bolted on, no ZUS fasteners allowed. Foam wheel covers are preferred.

4.21 LM's, CM's, TS's, C4 and Mini Stock must have a "Roll Over Vent Valve" installed in the Fuel Cell vent line. This check valve can be installed either internally in the bulkhead of the cell or can be an in-line check valve installed on the AN fitting or in the vent line itself.

4.22 Holley 26-342 Fuel Bowl Vent Tube; Spill Reduction Valves recommended for TS, CM and LM

4.23 Each Pit Stall MUST have a valid and working fire extinguisher all times. This is in addition to any onboard fire extinguisher or fire system in a race car.

5.0 FLAG PROCEDURES

5.1 YELLOW FLAG

When the yellow light comes on you will **slow down and hold your position. No passing allowed.** Any driver passing on the yellow flag will be put in the scratch position. If done a second time in the same race, he/she will be black-flagged. Cars must not break formation until the green flag is dropped during the start or restart of a race. The lead car will keep the pack slowed down until the race resumes. Laps run under the yellow will not count. No driver is to stop or get out of his/her car on the track surface while yellow or green light is on.

5.2 RED FLAG

When the red light comes on, cars must move over to the **inside of the track and stop within half lap of the track**. Failure to comply will result in suspension of driver from the remainder of that night's racing. There will be no movement of cars on the track while the red light is on unless instructed by the race director. **No driver shall get out of his/her car on the track surface unless explicitly told to by the race director or a speedway official**

5.3 GREEN FLAG

Indicates the race in progress. All cars on the track may race as soon as the green flag is displayed.

5.4 CROSSED FLAGS

Two furled flags, held in the crossed position resembling the letter "x" indicate the leader has passed the halfway point in the race.

5.5 WHITE FLAG

Indicate that the lap in progress is the final lap under present conditions. When used prior to start/restart of a race, a white flag indicates that a green flag will be displayed for the start of the next lap.

5.6 BLUE FLAG WITH DIAGONAL STRIPE

Yield right of way to overtaking car. A driver receiving this passing room signal is thereby informed that he/she is or shortly will be impeding a faster car (not necessarily the leader), which is about to overtake and pass him/her. He/she is further instructed hold his/her line on the race track and race competitively but not aggressively. If the starter feels that the driver being lapped is unable to hold his/her line and or is racing too aggressively endangering the position of the driver doing the passing the starter may issue a black flag. "Passing Room" flag will be used at the starter's discretion and not necessarily given to all lapped or slower moving cars. Cars, which in the starter's opinion are moving too slowly and may present an impediment, will be asked to leave the track over the one way radio by the race director.

5.7 BLACK FLAG

Leave the track as quickly and as safely as possible. A driver receiving this signal is informed that, for some reason or set of circumstances, he/she **must cease racing immediately and leave the track without stopping or otherwise impeding traffic**. Disqualifications may be result of a rule infraction such as unsportsmanlike driving, or failure to obey the starter's signals.

If a driver receives the Black Flag during a qualifying event, they are disqualified for that individual race only and will receive no points for that race. They will however be allowed to participated in the remainder of the race program. Any driver receiving a black flag in a feature event will receive no points and no money for the feature event.

5.8 CHECKER FLAG

Indicates that the lap being completed is the last lap of the race for each car receiving the checker flag as it crosses the start-finish line. The winner of the race shall be the first car, which crosses the line having completed the designated number of laps for that event. Circumstances may arise that cause the race to be ruled a complete race before the originally designated number of laps have been run. If the race is halted for these reasons it will be at the discretion of the track officials and the head scorer will determine the finishing positions according to the scoring procedure.

6.0 TEAR DOWN PROCEDURES

6.1 Drivers requesting to complete a tear down on another car must be a registered driver and have a registered car in the respective class. Driver requesting to carry out the tear down must have attempted to qualify on the night he is requesting the tear down.

6.2 Drivers will be given one tear down credit per season. With this credit drivers will be permitted to carry out 1 tear down. If at the end of the tear down there is a legal verdict, the driver doing the tear down will lose their credit. If the tear down results in an illegal verdict, the driver doing the tear down will keep his credit and be able to continue with a future tear down if desired.

6.0 The tear down request must be made in writing on the official tear down form to the Pit Director. The request must be completely filled out specifying the level of tear down and required amount of money (see listing below). The official tear down request forms can be attained from a Pit Director at a convenient time.

6.3 Tear down forms must be handed to the Pit Director no later than 10 minutes after the drop of the checker flag. No other official will accept the tear down form. Pit Directors stopwatch will be official.



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6.4 In order for a driver to do a tear down he/she must be within 3 positions in the point's standings or within 3 positions in the respective feature race finish.

6.5 Any driver found illegal will lose all money won for the complete race event unless specified elsewhere in this rule book, any driver being found illegal a second time will lose all points and money for the event and be subject to a \$100.00 fine prior to being allowed to return.

6.5 Any driver leaving after being told that he/she is going to be torn down or does not permit a tear down will be assumed illegal with the above applying as well as an extra \$250 fine being applied

6.6 The driver doing the tear down may designate a representative for him/her in the garage with the complete understanding that designate will have full authority to act on his/her behalf and accepting any punishment or ruling that may be made against him/her. Any designated representative MAY NOT be another driver in his'/her respective class.

6.7 Either driver or their representatives will be allowed one additional person to accompany them into the tear down area.

6.8 The track officials reserve the right to determine if any other parties need to be present during the tear down.

6.9 It is the responsibility of the driver doing the tear down to prove illegality with proper documentation and specifications. (Specifications other than the original manufactures **must be pre-approved** by officials).

Tear Down Procedures Continued

6.10 During a tear down the driver or mechanic performing the tear down will not be permitted to view the part number or measure the protested engines camshaft.

6.11 Brighton Speedway reserves the right to do mandatory tear downs at random at any time during the course of the season. Normal legality rules will apply. Same penalties apply whether track or driver initiates the tear down.

6.12 In the event Speedway officials perform a mandatory tear down on any night and a legal outcome is decide there will be **NO reimbursement for a rebuild kit or gaskets.**

6.13 The following tools will be supplied by the Speedway:

- Holley 4412 test tools for Pro Stock and Canadian Modified carburetors.
- Cylinder head CC checker.

6.14 Other speedway tools may be supplied by the speedway but we advise you to check first prior to proceeding for clarification. All other tools and supplies must be provided by driver and mechanic carrying out the tear down.

6.15 There will be a time limit of 2.5 hours for each tear down. Time limit will be waived if driver being torn down wishes to disassemble own engine. However Speedway officials may intervene if time becomes a factor.

6.16 If oil pan is to be removed during tear down on a RWD car, engine must be removed from the car.

6.17 The driver doing the tear down or cylinder head inspection is **RESPONSILBE** for draining all the fluids, **PROVIDING** the drain buckets, **PROVIDING** fluid removal containers and **PROVIDING** absorb-all (cat litter) to clean up the mess. The removed fluids **WILL** be taken away with the driver doing the tear down at the end of the tear down. This process will not be included in the allotted tear down time.

6.18 There will be no Alcohol allowed by any participants before or during a tear down.

TEAR DOWN DESCRIPTION AND FEES

TEAR DOWNS WILL INCLUDE:

Engine Block, Intake, Cylinder Heads, Internal Components, Clutch, Flywheel, Transmission.
(External components are included but subject only to mechanical protest penalties)

TEAR DOWN FEES \$350.00

- \$250 going to the protested car if found LEGAL
- \$250 returned to the protester if car found ILLEGAL
- \$100 to Speedway

CYLINDER HEAD and INTAKE INSPECTION:

May remove the intake and one cylinder head. Inspection includes the head, intake, valves, valve springs, spring hardware, push rods and rockers.

CYLINDER HEAD INSPECTION FEES \$150.00

- \$125 going to the protested car if found LEGAL
- \$125 returned to the protester if car found ILLEGAL
- \$25 to the speedway

7.0 PROTEST PROCEDURES

7.1 **VISUAL PROTEST** may be made to the Pit Director in writing on a Visual Protest Form, at any time throughout the night up until 10 minutes after the drop of the checker flag for the class respective feature race. Forms must be completed thoroughly and be made specific, so they are not left for interpretation. Appropriate action will be taken according to the seriousness of the infraction. Any time a tool or mechanical device has to be used by an official to determine car rule compliance it is not a visual inspection. (Tape measure is excluded).

- **Visual Protest Fess: \$25.00**
- o \$25 to speedway

7.2 **MECHANICAL PROTEST** may be made to the Pit Director in writing on a Protest Form, at any time through out the night up until 10 minutes after the drop of the checker flag for the class respective feature race. Forms must be completed thoroughly and be made specific so they are not left for interpretation. Driver doing the protest must supply all necessary equipment. If protested driver is found illegal he/she may lose all points and money earned on that race meet **or be imposed any other penalty** at the discretion of the officials. In order for a driver to do a mechanical protest he/she must be within 3 positions in the point's standings prior to the start of the feature.

- **Mechanical Protest Fee: \$50.00**
 - o \$50 to speedway
- **Clutch and Flywheel or Rear End Inspection Protest for RWD \$75.00, FWD \$150.00**
 - o \$25/\$100 going to the protested car if found legal.
 - o \$25/100 returned to the protestor if found illegal.
 - o \$50 (protest amount) to speedway.

7.3 Brighton Speedway reserves the right to do mechanical protests at random at any time during the course of the season. Normal legality rules will apply.

7.4 A protest of race results must be made to the pit marshal within 10 (TEN) minutes of the official announcement/posting of the finishes. This will be strictly enforced. Only driver directly affected may protest and must be made to the pit director. A recheck of the scoring will be made if necessary, and these results will be considered final. Absolutely no drivers allowed in the scoring booth at any time.

7.5 No individual or representatives or any group will be recognized in any discussion or dispute unless they are a registered driver and a signed protest has been submitted to the proper track official.

7.6 Speedway reserves the right at any time to reject a component that it feels would add an expense to the class or unfair advantage to a driver.

7.7 Maximum amount of protests by any driver in any one race event will be two protests of any kind.

8.0 POST RACE TECH

8.1 Post race tech is at the discretion of Speedway officials. Only 2 representatives from each car may be present. Penalties may include but are not limited to points and pay out.

Amendments, revisions and/or clarifications may be made to the rules or procedures as dictated by unforeseen circumstances which may arise. The management also has the right to refuse any protest if they feel it is unnecessarily being used for harassing reasons. All fines and/or suspensions will be in effect at all race events or functions. **The interpretation and invoking of the rules published herein shall be determined by the speedway officials only in their professional discretion.**

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WEBSITES

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APPAREL

The advertisement displays various examples of work: a laptop and smartphone showing a website for 'HOLLYWOOD HOLDAWAY', a hero card for 'LINCOLN' and 'SANDER COCK', and a t-shirt with 'TRIPLE CROWN' and 'ONSWEKEN' designs.



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CANADIAN MODIFIED CLASS

General Rules, Race Procedures, Safety Procedures and Tear down Procedures in the front of this book apply to all classes.

Cars running the full UMP or IMCA rules package will be allowed to run specials and some weekly events with pre-approval by Speedway Officials

Note: Cars that do not meet the rules due to 2021 Chassis and Body rule changes need to discuss with Speedway Officials to decide if they qualify for a "grandfather" clause.

1.0 CHASSIS AND FRAME

1.0 Only factory production full 1965 or newer parallel American passenger car frames will be permitted. The frame must be complete in front of firewall. Cutting of the frame in any way in front of firewall will not be permitted.

1.1 The frame must remain unaltered and must meet the requirements of the attached drawing.

1.2 The Right-side frame rail may not be altered and/or raised and/or moved from the stock OEM location in any manner and must measure a maximum of six (6) inches from the ground to the bottom or lowest edge of the frame rail.

1.3 The front cross member may be notched to allow clearance for the radiator only. The cross members must be plated to retain complete box configuration.

1.4 Jeep, Bronco, 4-wheel drive, front wheel drive or sports car frames will not be permitted.

1.5 The minimum wheelbase permitted will be 108" inches. The maximum wheelbase permitted will be 112" inches. No tolerance – measurements are as posted

1.6 Fiberglass lift bars and/or rear torsion bars will not be permitted.

1.7 Any frame may not be widened or narrowed and must be able to support the roll cage on both sides and must be full and complete on both sides.

1.8 The minimum frame and body height from ground is four (4) inches.

1.9 The front bumper must be mounted to the ends of the frame with the bottom loop parallel to the ground.

1.10 A minimum of 1" diameter tubing and/or pipe is required to for towing of the vehicle.

1.11 The rear bumpers must be constructed of tubing and/or flat stock. All rear bumpers and/or tubing utilized in the fabrication of the rear bumper assembly must have an end cap. The rear bumpers must protect the fuel cell.

1.12 The bumper(s) (front and rear) when measure from their center(s) must have a minimum of 25" inches of ground clearance. There will be a two (2) inch tolerance, plus or minus regarding this dimension. The rear bumper and/or the nerf bars must not extend beyond the width of rear tires. All bumpers must have round edges. Sharp edges on any bumper and/or bumper surface will not be permitted.

Chassis and Frame Continued

- 1.13 The brace bars forward of the roll cage must not extend higher than stock hood height.
- 1.14 The rear push bars must not extend beyond six (6") inches of the rear quarter panel(s).
- 1.15 All side bars and bumpers must be capped on ends.
- 1.16 The maximum overall width of the car, front and/or rear, shall not exceed 83.78" inches when measured from the outside of the tire tread on both sides of the vehicle. A maximum one (1") inch spacer between the hub and wheel will be permitted. Steel and/or aluminum spacers will be permitted, provided the overall tread width of the vehicle does not exceed 83"-inches.

2.0 ROLL CAGE

- 2.0 All cars must have a roll cage. The roll cage must be approved prior to competition and it must be welded to the frame. The driver's head must not protrude above the top of the roll cage with the driver's helmet on. The top of the roll cage must extend above the top of the driver's helmet by a minimum of one (1) inch. The entire roll cage and all roll bar pieces must be fabricated from magnetic steel a minimum of .095" in thickness.
- 2.1 The driver and right-side door bars must be parallel to the ground and located perpendicular to the driver. There must be right side door bars. The side bars must be welded to the front and rear of the roll cage. No brazing or soldering is permitted. The door bars must have a minimum thickness of .095" and must have a minimum of 1-1/2" diameter. On the driver's side, there will be a minimum of three (3) door bars.
- 2.2 Foot protection is mandatory. One piece of magnetic steel tubing, a minimum .095" in thickness, must be fastened and/or welded across the back of the engine with vertical tubing a minimum of .095" thick.
- 2.3 The door side roll bars are mandatory and must extend into the door panels.
F. The roll cage must consist of continuous hoops that are no less than 1-1/2" in diameter and must a minimum thickness of .095".
- 2.4 The roll cage must be mounted securely (welded) to the frame in a minimum of 6 (six) places.
H. The roll cage must consist of a configuration of front and rear hoops connected by tubing on the sides and/or side hoops. Roll cage must be securely supported and braced. Gussets to brace any portion of the roll cage will be permitted.

3.0 BODY (See drawing diagram for reference – there may be dimensions referenced in the drawing diagram that are not referenced in the written rules and vice versa. All dimensions whether written and/or referenced in the drawing diagram dimensional are rule(s).

- 3.0 All bodies must follow dimension(s) and the appearance as shown in diagram. The body shall be the fabricated modified body from approved vendors. **The body must be symmetrical, including but not limited to doors, nose, sail panels, spoiler supports, etc.** Composite door, quarter & sail panels will be allowed.
- 3.1 The front windshield and rear window support must be stock appearing. The front window support(s) may have a clear Lexan support of no more than 16" inches including the roof post at the bottom and going straight to the roof. The support must be securely fastened in a positive manner.
- 3.2 The front roof post must be in a straight line from top to bottom.
- 3.3 The sides of the engine compartment must remain open. Any side panels in the engine compartment area will not be permitted. A five (5") inch maximum drop of the hood on either side of the hood will be permitted and it must be enclosed at the rear of the hood. The firewall on driver's side must meet the front of the door. The firewall must be a minimum of 16"-inches in length from top-to-bottom.
- 3.4 The top of the doors must remain in line with the rear engine plate. The bottom of the doors may extend a maximum of eight (8") inches past the motor plate toward the front of the vehicle. Any material that exceeds 8"-inches forward of the engine plate will not be permitted.
- 3.5 Right panel from top of door to bottom of door may not be no more than 3 inches and must be mounted straight inside the bottom.
- 3.6 A maximum one (1") inch flange on the outside edges (left and right side vertically) for the purpose of strengthening only will be permitted on the front nose only. Side fins and/or flanges of any kind along the length of the entire car (hood, front, and/or roof) will not be permitted.
- 3.7 A maximum of one (1) stone deflector, mounted only on the right side of the car, for rear mounted oil pumps, oil filters, and for the main oil tank will be permitted. The deflector may be made of steel, aluminum, or heavy gauge wire. The cover may only be mounted near the unit and is designed to protect with a maximum size of eighteen (18"x18") inches square and mounted only from the upper right frame rail to the lower right frame rail.
- 3.8 The minimum roof height is 42" inches with a maximum height of 52" inches.
- 3.9 The minimum deck height is 28" inches with a maximum height of 40" inches.
- 3.10 Deck height should measure within 1"-inch from side-to-side.

- 3.11 All cars must have a car number on both sides and the roof that are at least 18" inches in height and 14" inches in width.
- 3.12 The driver's last name must be displayed under the car number in a legible font that is a minimum of eight (8") inch high letters.
- 3.13 A minimum of two (2") inches of tire clearance from the body is required.
- 3.14 The minimum ground clearance will be four (4") inches.
- 3.15 The measurement from center of the hub to the rear top of deck will be a minimum of 34" inches and a maximum of 48" inches.
- 3.16 **The maximum body width at the body's widest part will be 72" inches.** The minimum of width at the body's narrowest part will be 53" inches. The top of the doors may have a maximum width of 72" inches. The bottom of the doors may have a maximum width of 73" inches. Q. On the left side, the rear of the door may flare out six (6") inches for tire clearance.
- 3.17 The bottom of the flare must be curved and rounded not pointed in any fashion.
- 3.18 The maximum rear width of car must be 67" inches at the widest point.
- 3.19 The maximum roof width will be 50" inches with a minimum of 44" inches.
- 3.20 The maximum roof length will be 56" inches with a minimum roof length of 41" inches.
- 3.21 The maximum height on rear roof post vertical is 3" inches.
- 3.22 The nose piece must not exceed 45"-inches in width, centered between the frame rails and must not extend past the leading edge of the front bumper. The nose piece must have a minimum of six (6") inches from the ground to the bottom edge of the nose piece. W. The MD3 D3 Modified Aero Valance is approved.
- 3.23 Rear spoilers [will be permitted to a maximum of 3"](#).

INTERIOR and DRIVER COMPARTMENT

- 3.24 All windows must have a minimum 11"-inch vertical opening. The entire interior must have a minimum of 11"-inches of clearance from the interior deck to the roof and all sections of the roll cage. The minimum window opening will be 12" inches. The interior deck sheet metal to the right side of the driver from the back of the seat forward should be fabricated on an angle from the driveshaft tunnel to the right-side window to promote ease of exit for the driver through the right side and or front window opening.
- 3.25 The drivers compartment must be sealed from the engine and the race track. The rear bubble of the hood should be closed and sealed from the driver.
- 3.26 Interior decks must be flat from side to side. Interior decks may have a up to a total of 6"-inches of "rake" from the most forward part of the deck to the most rearward part of the deck as described below. Interior deck from the rear of the seat forward may be flat or have up to 3"-inches of rake. The interior deck from the rear of the seat to the rear of the car cannot exceed 3"-inches of rake regardless of how much rake the front portion measures. The rake of the rear portion of the interior deck must be in a straight flat line from the rear of the seat to the rear of the car.
- 3.27 Any car in competition must have right side door bars as part of the roll cage. Please reference I. 22.11 – B in the roll cage section of the 2019 DIRTcar UMP Modified Rule Book.

ROOF & ROOF SUPPORT (see drawing diagram for reference)

- 3.28 All roofs must be full size. Half-moon roofs will not be permitted. The rear edge of the roof may not be mounted more than 12" (twelve inches) past the rear axle. The roof must be symmetrical.
- 3.29 Wings, roof spoilers or ground effects of any kind will not be permitted.
- 3.30 All flat type roofs will be allowed a maximum 3"-inch difference in height between the front of the roof and the rear of the roof. This measurement may be taken from the interior and/or the ground. The rear of the roof must maintain the same height from side to-side.
- 3.31 All curved type roofs will be allowed a maximum of 5"-inches in the front and 2"-inches in the rear. The break in the roof must be within 6"-inches from the center of the roof. E. E. Wedge roofs will not be permitted.
- 3.32 Dished roofs will not be permitted. Roofs must be rounded side-to-side. Bead rolls will be permitted.
- 3.33 The rear roof post must begin where it meets the back edge of the roof. The rear roof supports may not extend past the front side of rear upright of the roll cage.
- 3.34 The side vents on the front roof posts may extend rear ward from the lowest point, a maximum of 16"-inches, then at a 90-degree angle to roof for one (1) configuration, side vents must meet the outside of the car.
- 3.35 **The roof side (sail) panel window size must be 10"-inches x 15"-inches and must match drawing number - 2- side view.** [The roof side \(sail\) panels must be the same design, length \(top and bottom\) on both sides of the car. Sail panels must be a straight line from top to bottom on the front edge. Outward bowing of the sail panel will not be permitted.](#)
- 3.35(a) When using the long roof side (sail) panels, They must be mounted in the same way as described above.

- 3.36 If the long roof side (sail) panel is being used a 3"-inch radius will be permitted.
- 3.37 Any sun shield must be on hinge for easy exiting, taping will not be permitted. The window opening must remain at a minimum of 12"-inches.
- 3.38 All race cars in competition must meet the measurements illustrated in the DIRTcar drawing.
- 3.39 There are only 4 (four) approved roof supports to choose from.
- 3.40 The roof should be single thickness of material. The roll bar hoops must be exposed for inspection.
- 3.41 If the roof is curved then a level will be placed on the top of the car and if the roof is 5"- inches or less in the front and 2"-inches or less in the rear and the break is in the middle then the roof will be permitted.
- 3.42 The roofs must be stock appearing. The roof must maintain the same height on both sides. If a DIRTcar and/or local track official determines the roof does not meet the requirements as stated above then a penalty as determined by the official may be determined prior to competition, additional weight and/or other penalties, unless the roof can be returned to within the parameter as stated above prior to competition in the event that the roof was inspected for.

FRONT SUSPENSION / STEERING

Suspension/Steering designs and applications are constantly evolving. Although the intent of the suspension/steering rules are an attempt to accommodate the majority of suspension/steering and suspension/steering component designs and applications currently being used in competition, the rules cannot be absolute. Any and all new designs or modifications to an existing suspension/steering and/or suspension/steering component must be communicated to and approved by DIRTcar Racing before being used in competition.

Unless otherwise specified, suspension/steering components must be manufactured using magnetic steel only. This includes but is not limited to, shocks, hubs, all control arms (A-frames), axle tubes, tie rods, etc.

4.0 FRONT SUSPENSION

- 4.1 All front suspension and steering components (except the upper control arm mounts) must utilize the original O.E.M. suspension and steering component mounting holes. All mounting holes must remain a single round hole as produced by the O.E.M. Adjustable mounts and/or "slugs" will not be permitted.
- 4.2 Weight jacking bolts (wedge bolts) may be added to O.E.M. frame but, must be located within 1 (one) inch of the original centerline of the O.E.M. front spring location.
- 4.3 Unless otherwise specified, the front suspension components must be O.E.M. serviceable parts and originate from the same O.E.M. frame manufacturer for that application.
- 4.4 Unless otherwise approved, only O.E.M. and/or O.E.M. replacement type passenger car spindles will be permitted. Spindle O.E.M. manufacturer and model is optional. Fabricated spindles will not be permitted. Spindles must not be altered. O.E.M. spindles utilizing a removable steering arm must use the same O.E.M. steering arm originally supplied with the type and model of spindle being use or a fabricated steering arm that is an exact match (length, mounting holes, etc.). If a fabricated steering arm is used, competitors must be prepared to supply an O.E.M steering arm for an inspection comparison.**
- 4.5 As of 01/01/2018 the following approved multiple piece aftermarket spindles may be used for competition. **a. Speedway Motors: 91034313, 91034501/2, 91034511 b. Afco: 1063435L/R c. Argo manufacturing: RP-929 (AMC), Mustang II**
- 4.6 Alterations and/or relocation of the lower control arm mounts will not be permitted. Lower control arms may be O.E.M. lower control arms for the frame being used or DIRTcar approved fabricated lower control arms, which are mounted in the O.E.M. location and maintain the O.E.M. dimensions. Lower control arms must be the same length when comparing to O.E.M. right and left. The lower coil spring seat must be an integral welded component of the lower control arm. If using spherical rod ends (heim joints) as lower control arm mounts, the spherical rod ends (heim joints) must be welded to the lower control arm to prevent adjustment. Competitors must be prepared to supply an O.E.M lower control arm for an inspection comparison. All fabricated lower control arms must fit the Speedway lower control arm fixture(s).
- 4.7 Tubular-type upper A-frames will be permitted. The upper A-frame mounting locations may be relocated. Aluminum cross shafts will be permitted.

- STEERING

- 4.8 An O.E.M. steering system applicable to the O.E.M. frame assembly must be used. The steering system must consist of 1 (one) reciprocating steering gear assembly, 1 (one) pitman arm, 1 (one) idler arm, 1 (one) center steering link (drag link), 2 (two) inner tie rod ends or spherical rod ends, 2 (two) adjusting sleeves, and 2 (two) outer tie rod ends or spherical rod ends. Additional components, braces, rods, etc. will not be permitted.
- 4.9 O.E.M. or exact fit aftermarket replacement center steering (drag) link, pitman arm, and idler arm available at

auto part replacement venues will be permitted. Aftermarket components must match O.E.M. for all specifications, dimensions and design. Only single point round tapered mounting holes and O.E.M. tapered ball studs will be permitted.

4.10 **As of 01/01/2018 fabricated center (drag) links, braces, etc. will no longer be permitted.

4.11 Steel spherical rod ends (heim joints) and fabricated magnetic steel tie rod adjusting sleeves will be permitted. 5/8" or larger spherical rod ends and fasteners must be used. The inner rod end must connect to the center steering link using a stud that is tapered to fit the center steering link on one end with a straight 5/8" stud on the opposite end, or the center steering link may be drilled to accept a 5/8" bolt. The outer rod end may use a 5/8" grade 8 bolt with a bump steer spacer to connect to the steering arm. The O.E.M. steering arm may be drilled to accept a 5/8" bolt. Tie rod adjusting sleeves must be magnetic steel tubing.

4.12 The steering shaft and interior mountings may be fabricated. The steering wheel must remain on the left side of the vehicle. The steering shaft must have at least 1 (one) universal type joint to allow shaft to "fold" under impact.

4.13 All steering wheels must connect to the steering shaft with an approved quick release coupler. SFI certified steering wheel couplers are highly recommended.

5.0 REAR SUSPENSION

5.1 Rear suspension configuration used on current and new chassis(s) must be the design commonly known as four (4) link. Older cars currently competing with other rear suspension designs will be allowed to compete until further notification at the discretion of the Series Director.

5.2 The frame/roll cage structure must have integral welded mounting brackets for the attachment of rear suspension components. Frame suspension mounts may be welded or bolted securely (without any movement) to the frame/roll cage structure.

The only material used to fabricate frame suspension mounts that will be permitted is magnetic steel.

5.3 Axle Housing Mounts:

a. Aluminum birdcages will not be permitted.

b. Axle housing mounts may be a solid (welded) type or a floating type (birdcage) design.

c. Only one (1) axle housing mount per side will be permitted.

5.4 The final assembled axle housing mount must be a one (1) piece mount. When a floating type mount (birdcage) is fabricated using two (2) pieces, the two (2) pieces must create a common one (1) piece pivot (barrel). The two (2) pieces must be fastened or welded together to prevent independent movement of the two (2) pieces. The axle housing mount must attach directly to the axle tube with clearance only to permit rotation of the entire mount. Fore, aft or vertical movement of the mount or the axle housing within the mount will not be permitted.

5.5 Only 2 (two) radius rods per side will be permitted to be connected from the axle housing mounts (birdcages) to the frame. Radius rods must be magnetic steel. Spring rods or any type of radius rods that change length dynamically are not permitted. (See Brakes 15.7.6D for floating caliper specifications)

5.6 Independent rear suspensions will not be permitted.

5.7 In regard to swing arm and/or Z-Link suspension, these suspension types are permitted. The shock on a swing arm or z-link rear suspension may mount to the bird cage or bottom radius rod. Top and bottom solid links must be mounted on hiems and run in the opposite direction of bird cage.

6.0 AXLE HOUSING & REAR DIFFERENTIAL

6.1 Ford 9" style steel full floating rear ends allowed.

6.2 Quick Change rear ends allowed with one piece steel axle tubes maximum 3" only, Standard 4.86 10" inch ring gear only, aluminum or magnesium center sections allowed. "Live Axle" rear ends will not be permitted

6.3 Final drive gear ratio of not more than 6:50 to 1 for all competitors.

6.4 Welded differentials or steel mini spools allowed, Aluminium full spools allowed in both quick change and 9" rear ends, open, limited slip, traction sensitive, Detroit lockers or any other device that allows independent wheel rotation will not be permitted.

6.5 Drive shafts must be steel only, no aluminium or carbon fibre.

Axle Housing and Rear Differential Continued

6.6 No gun drilled axles allowed, magnetic steel axles only.

6.7 Drive flanges, wheel spacers and lug nuts are allowed to be aluminium.

6.8 360-degree drive shaft hoop mandatory on all cars. To be placed 6" from front U-Joint must be made of 1/8" thick x 1" wide strapping.

7.0 Shocks and Springs

7.1 One steel non-Adjustable shock per wheel, rebuildable shocks permitted, schrader valves allowed for adjusting gas pressure only, No Bulb / No Canister type shocks permitted.

- 7.2 There will no longer be a maximum spend on shocks, HOWEVER there will be a claim rule on shocks. Claim rule for shocks will be \$250 USD (current day equivalent) for shock alone or \$350 USD (current day equivalent) for any coil over combination that is installed on a car.
- 7.3 Shock numbers be present and legible at all times, no altering the shock from the original manufactured design.
- 7.4 One bump stop, non-metallic is permitted. One bump stop cup/retaining device, which is visually open, is permitted. Shims are allowed.
- 7.5 Rear shock mounts may use heim joints and may use DIRT Modified style aluminium shock mounts,
- 7.6 Coil over shocks kits, the coil over kit may be aluminium.
- 7.7 Coil over eliminators allowed or sliders on rear suspension, no dummy shocks allowed.
- 7.8 Front springs must be steel springs only and must be at least 4.5 inches in diameter. One spring per side, no progressive springs. Spring rubbers allowed.
- 7.9 Rear Springs must be steel springs only and can be 2.5" or 5" diameter, One Spring per side, no progressive springs allowed on the four corners.

8.0 BRAKES

- 8.1 Adjustable brake valve, front to rear and right front shut off or limiter allowed.
- 8.2 Any aftermarket balance bar pedals allowed.
- 8.3 Four-wheel disc brakes allowed.
- 8.4 Rotors and Callipers may be intermixed between make and model for North American OEM's, Must be magnetic steel.
- 8.5 Rotors cannot be drilled to lighten. Rotors may be re-drilled for different bolt patterns or larger studs.
- 8.6 Callipers cannot be ground or lightened. Only one calliper per wheel.
- 8.7 No composite, titanium or other exotic metals or material products, parts or components allowed on the race car.
- 8.8 No Floating rear calliper style brakes

9.0 ENGINE POSITION

9.1 Engine mounting must be at least 72 inches from back of engine block mounting surface to centre line of rear end housing. Engine offset must be kept within 2 inches of centre line of front cross member.

10.0 GM CRATE ENGINE OPTION

10.1 **Sealed Crate Motor Only G.M. 88958602 Engine Allowed**

10.2 **ALL Crate Motors for Canadian Modified and Pro Stock must have BSP seals in them. Any new motor must be purchased through [Peter Smith Chevrolet in Belleville](#) and be sealed BEFORE the driver receives the motor. Any motor that does not go through [Peter Smith](#) is subject to inspection of any level prior to BSP officials sealing it.**

10.3 [Peter Smith](#) is an authorized dealer for Brighton Speedway. You must have copy of invoice with serial number and inspect engine prior to racing.

10.4 Serial number must remain on engine, No grinding or polishing of any kind is allowed anywhere on the castings.

10.5 Factory seals must remain on engine for new engines.

10.6 Authorized Repair or Rebuild for Brighton Speedway 602 Crate motors are Workman Auto repair

10.7 Any used engine Crate coming into the Brighton Speedway System is subject to inspection By Brighton Speedway Authorized repair facilities.

10.8 Any and all work to crate motors must be pre-approved by the speedway and must be carried out by one of the authorized repair facilities. No driver or crew may open up a crate motor period.

10.9 Brighton Speedway and the recognized repair facilities may approve an aftermarket part that may allow the repair facility to put into the crate motors. This is to save money on rebuilds and all parts will be stock replacement and equivalent.

10.10 A sealed crate motor is exempted from tear downs from other drivers. However if the Speedway feels there is cause they have the right to inspect the engine to any level.

10.11 No modifications except valve springs may be changed. Valve spring must be replaced with stock springs PN 10122811, that can be purchased from any GM dealer. The speedway will have a tool for checking the springs.

10.12 **Anyone caught tampering with sealed crate motor is subject to full suspension from competing in any Speedway event for a minimum 365 days.**

10.13 Any bolt on parts or components not specified above fall under the BSP built engine rules (i.e. water pump, fuel pump, etc.)

10.14 [ODCC sealed crate motors will be approved for use in both Modified and Thunder Stock](#)

11.0 BSP Built Engine

11.1 GM / Chevy 350CI two-barrel carburetted type engine only OEM block does not have to be of two- barrel type.

11.2 Not more than .060 plus clearance allowed over standard block. No turbo or super chargers. No aluminium blocks and heads. Maximum zero decking allowed.

11.3 Must have composition gaskets with a minimum compressed thickness of not less than .038". Metal head gaskets may be used but a minimum deck height of not less than .015" must be retained.

BSP Built Motor Continued

11.4 All casting numbers must remain unaltered and visible.

11.5 No coating of any parts (See piston notes below)

11.6 Stock flat top pistons with 4 valve relief, cast, forged or hypereutectic with compression ring groove of minimum 5/64" and for the engine being used. **No Coated pistons except for the following GM replacement piston part number Speed Pro/ Federal Mogul Piston FM H345NCP or H345DCP, Maximum .060.**

11.7 Stock cast rods only. Harden rods bolts allowed. **Alternative aftermarket stock replacement rods may be used the following part number Scat rod part number SCA3-1CR5700P.**

11.8 Only cast cranks stock for the make and model of engine. No steel cranks. **Aftermarket Scat stock replacement crank shaft may be used under the following Scat part number 9-10442 or Scat PN 9-10526.**

11.9 4 bolt main blocks allowed, their components must be off a two-barrel carburetted engine.

11.10 Do not have to use stock pulleys.

11.11 Any harmonic balancer with the exception of no fluid type balancers.

11.12 Balancing of engine allowed. When balancing one piston and rod assembly must remain untouched and stock

11.13 Screen kits on oil return holes allowed.

11.14 Fabricated oil pans allowed. Crank scrapers allowed. High volume oil pumps allowed.

11.15 Cooling fans must be steel one-piece fans only, any number of blades allowed.

11.16 Stud kits allowed.

11.17 No aluminium water pumps allowed.

11.18 No Oil Coolers allowed

12.0 CAM SHAFT

- 12.1 Hydraulic camshaft only. No roller, flat tappet or overhead cam.
- 12.2 Cam buttons allowed, and degreasing allowed.
- 12.3 Anti pump-up lifters allowed.
- 12.4 Double roller timing chains allowed, no gear driven cam shafts.

13.0 CARBURETION & INTAKE MANIFOLD

- 13.1 Two barrel carb, Holley 500 (**No. 4412**) must retain 1-11/16" throttle plates and no casting modifications. Holley Ultra 4412 XP (0-4412HBX) is not allowed.
- 13.2 **No modifications** with the exception of choke plate removal, changing of jets, accelerator pump cam, power valve, squirter nozzle, and fuel bowl vent anti-splash kits may be changed - all parts must be present.
- 13.3 Metering block part numbers are limited to the following 11180, 5924 and 5925. Metering block measurements can be found on pg. 23
- 13.4 No vacuum hoses from carb. All vacuum ports must be capped or blocked.
- 13.5 A maximum of one 1-1/2" thick adaptor plate including gaskets allowed.
- 13.6 **Performer aluminium Intakes, the following are the part numbers are mandatory with no exceptions:**
- 13.7 - GM 2101
- 13.8 Note: Early style GM 2101 intakes with Edelbrock logo cast directly in front of the carburettor pad and parallel to the pad and the performer logo on top of the #8 runner adjacent to the 3/8"NPT vacuum provision will NOT be allowed
- 13.9 No machining or modifications. Blocked port or propane gaskets allowed.
- 13.10 Installation of electronic tuning devices on car of any kind, prohibited.
- 13.11 Heat shields and air boxes allowed from the bottom of the breather and above the carburettor.
- 13.12 Holley 26-342 Fuel Bowl Vent Tube; spill reduction valves recommended

14.0 IGNITION

- 14.1 Must maintain stock firing order.
- 14.2 Crate Motors must run stock distributor as it comes from GM, Only modification is distributor vacuum advance lock outs Pn: CP725 and CP726 are allowed.
- 14.3 Built motors original stock or stock replacement HEI distributors allowed. Any module allowed, Curve kits allowed however all parts must be present. Distributor Vacuum advance lock outs allowed Pn: CP725 and CP726.
- 14.4 Ignition rotor height may be shimmed.
- 14.5 MSD Rev Limiter boxes PN MSD 8727CT only are allowed however they must remain stock from MSD and unaltered. BSP reserves the right to confiscate MSD boxes for inspections as required.
- 14.6 No traction control devices of any kind.
- 14.7 No Lithium batteries of any kind allowed

15.0 CYLINDER HEADS

- 15.1 Only cast iron cylinder heads allowed.
- 15.2 GM Engines may use World Products Cylinder Heads Part Number WP 043600 or DART heads PN 10024360.
- 15.3 Chevy 300hp heads or equivalent not allowed, No angle plug heads.
- 15.4 Ford 351 must use two-barrel heads.
- 15.5 All casting numbers must remain unaltered and visible.
- 15.6 Absolutely no de-burring, grinding, or polishing of intake and exhaust ports allowed.
- 15.7 Three angle valve seats allowed. Cutting of three angles must be on valve seat, third angle face must be no longer than .250". No cutting into valve body.
- 15.8 No un-shrouding of the valve in the combustion chamber allowed.
- 15.9 When cutting or grinding valve seats on GM heads there can be no cut marks or grinding marks in the side of the combustion chamber. World Product heads have the valves un-shrouded and combustion chamber sides cut, this is allowed but there is to be no additional cutting in these areas
- 15.10 Chevy cylinder heads and World Product heads are restricted to **74cc** minimum chamber volume.
- 15.11 Ford cylinder heads are restricted to **58cc** minimum chamber volume.
- 15.12 Worn spring seats may be machined and shimmed.
- 15.13 Bronze guide inserts allowed.
- 15.14 Rocker studs may be pinned, locking rocker nuts allowed.
- 15.15 Hardened push rods allowed, Push rods can be any length.
- 15.16 Hardened head bolts allowed.
- 15.17 Stud kits are allowed. Screw in rocker studs with hex allowed.
- 15.18 Guide plates allowed.
- 15.19 Long slot rockers of stock dimension may be used.

16.0 Valves and Valve Springs

- 16.1 Maximum valve diameter 1.94 intake and 1.50 exhaust.
- 16.2 Stainless steel valves allowed.
- 16.3 Only one angle on the valve face, NO back cutting and NO under-cut valves
- 16.4 Any single dampener type valve spring must be continuous equal diameter from top to bottom, can be used, but must fit in stock spring pocket with no diameter machining.
- 16.5 Valve spring retainers and hardware must be of conventional style, no light weight, drilled or convoluted retainers.

17.0 EXHAUST

- 17.1 All cars must have exhaust pipes that exit outside the body or behind the driver if the exhaust exits inside the body.
- 17.2 The exhaust pipe should be securely mounted and have no sharp edges nor protrude outside the bodyline.
- 17.3 Must not have any holes.
- 17.4 Headers allowed. No ceramic or other coatings allowed.
- 17.5 No Tri-Y Headers will be allowed.
- 17.6 NOTE: Mufflers are not mandatory, however Brighton Speedway has picked the muffler that will be implanted in the future. If you want to run mufflers now, you must run Schoenfeld IMCA 609, IMCA 930 or IMCA 935 mufflers. No other mufflers or resonators allowed.

18.0 TRANSMISSIONS & CLUTCHES

- 18.1 Any stock type transmission.
- 18.2 Must have and operate in all gears, no aluminium manual transmissions.
- 18.3 Automatic transmissions must use V-8 torque converter of stock diameter and weight for the model
- 18.4 Six or eight cylinder clutch and flywheel allowed. (Stock steel units only.)
- 18.5 No turned, drilled, aluminium or special speed equipment flywheels or clutches permitted.
- 18.6 No multi disc clutches,
- 18.7 Hydraulic release clutches allowed.
- 18.8 FLYWHEEL, CLUTCH AND PRESSURE PLATE, bolts included must weigh and minimum 40 lbs, CRATE MOTOR weights for the same assembly listed above are 36 pounds.
- 18.9 Sema 6-1 Flywheel shield or a 1/8" or more in thickness steel scatter shield that is positioned between the floor and aluminium bell housing, covering the top part of the bell housing, 180 degrees around, is required on all cars with manual (stick shift) transmissions.

19.0 FUEL SYSTEM

- 19.1 Fuel cells mandatory. Must be securely mounted in the chassis with bars wrapped around the cell 360 degree
- 19.2 Must be enclosed in a minimum 20 gauge steel box and properly protected from rear impact.
- 19.3 Filler necks to be kept as short as possible.
- 19.4 Any stock type mechanical fuel pump that fits with no modifications allowed. No electric fuel pumps.
- 19.5 **NO POOR INSTALLATIONS**
- 19.6 Fuel cell must be equipped with a roll over check valve in the vent line or external valve must be added.
- 19.7 Fuel line may be wrapped with flexible heat wrap, no fixed metal ducting or baffles.
- 19.8 **If Drivers want to run race fuel the ONLY race fuel allowed will be VP Fuels D98 sold by the track or designated resale agent. [No E85 Ethanol](#)**

20.0 TIRES AND WHEELS

- 20.1 Only Hoosier RC4 tires permitted. Tire sizes are 28/10.5/15 and 29/10.5/15.
- 20.2 Any Steel wheel maximum width 10". No wide five rims.
- 20.3 One bead lock rim allowed on the car at any time, bead lock rings must be steel.
- 20.4 Wheel spacers allowed, steel or aluminium, Aluminium lug nuts allowed
- 20.5 Tire cutting allowed.
- 20.6 No tire compound altering chemicals. BSP officials have the right to take a tire sample at any time during an event.
- 20.7 [During special events when UMP are permitted, they must run hard tires.](#)

21.0 WEIGHT

- 21.1 Minimum weight with the driver 2600 lbs.
- 21.2 Subject to be weighed at any time. The weight of Brighton Speedway scales will be official.
- 21.3 All added weight must be securely bolted to a main member of the chassis.

Anything not being specified as being allowed must be stock and in stock location.



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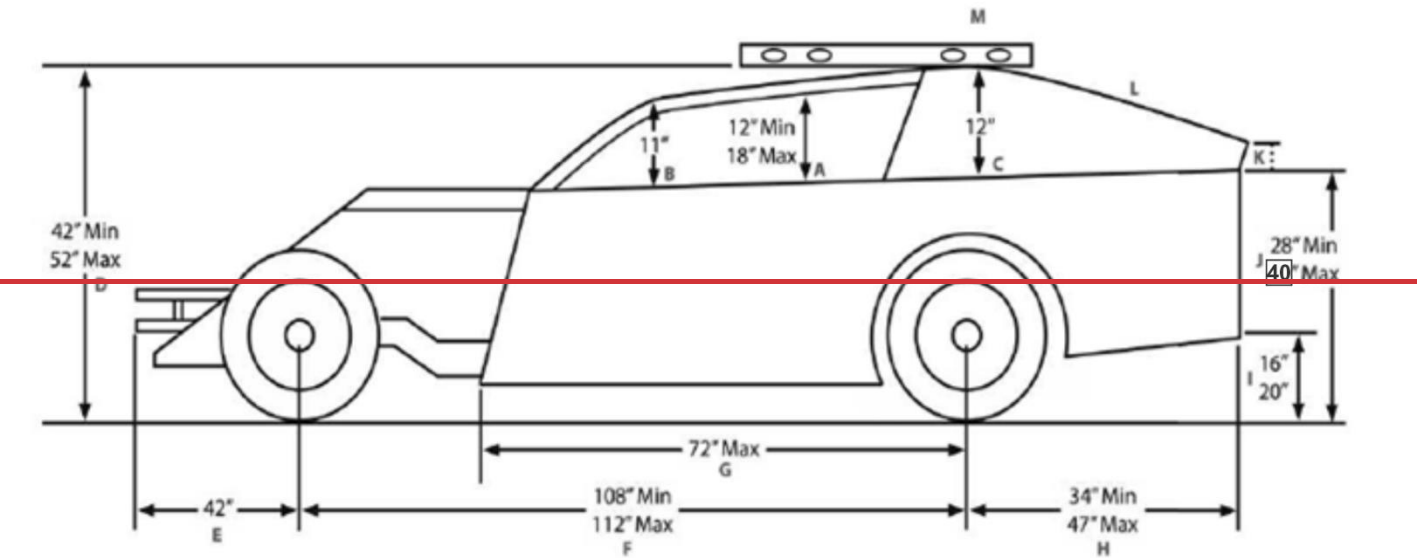
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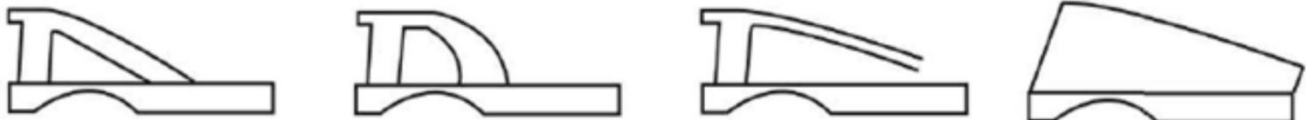
2025 Canadian Modified Body Rules

- A. Window opening - 12" Min - 18" Max
- B. Front edge of roof to interior deck - 11" Min
- C. Rear edge of roof to interior deck - 12" Min
- D. Highest point of roof to ground - 42" Min - 52" Max
- E. Center of front wheel to front edge of front bumper - 42"
- F. Wheelbase: Center of rear wheel to center of front wheel - 108" Min - 112" Max
- G. Center of rear wheel to front edge of door - 72" Max

- H. Center of rear wheel to rear edge of quarter panel - 34" Min - 47" Max
- I. Center of lowest bar on rear bumper to ground - 16" Min - 20" Max
- J. Deck height to ground - 28" Min - 38" Max
- K. Rear sail panel height 2" Max
- L. Sail panel curve from rear of roof to rear of panel - 2" Max
- M. Sail panel bow from bottom to roof - 2" Max

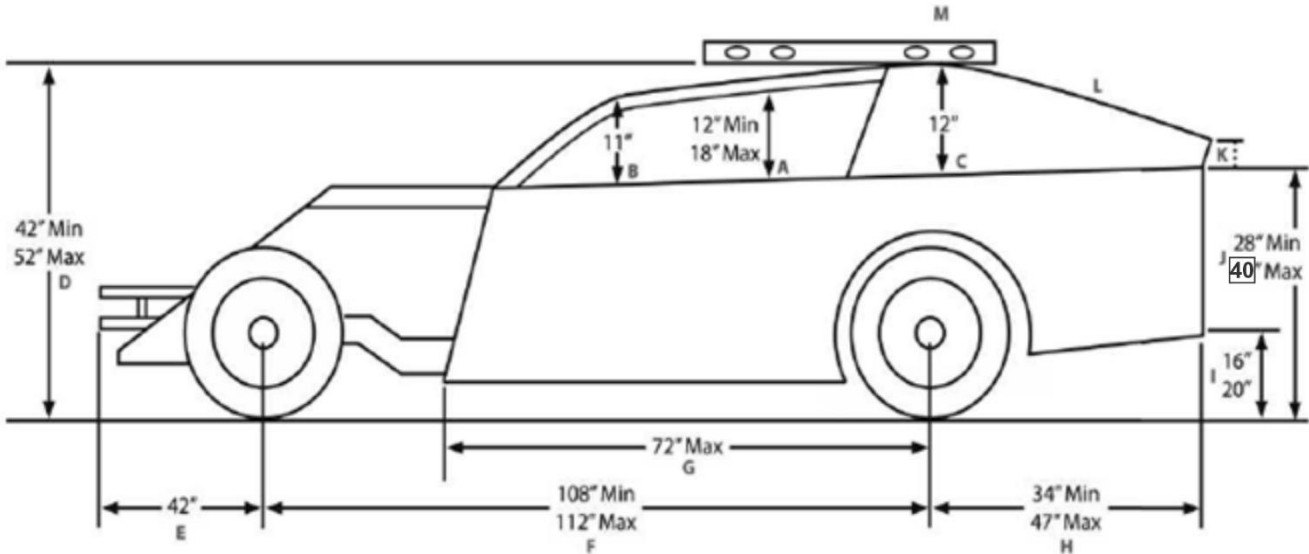


ACCEPTABLE ALTERNATE ROOF POSTS
(Window openings must be same on both sides)

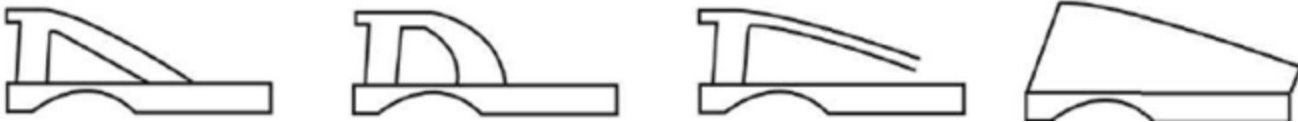


- A. Window opening - 12" Min - 18" Max
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- C. Rear edge of roof to interior deck - 12" Min
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ACCEPTABLE ALTERNATE ROOF POSTS
(Window openings must be same on both sides)

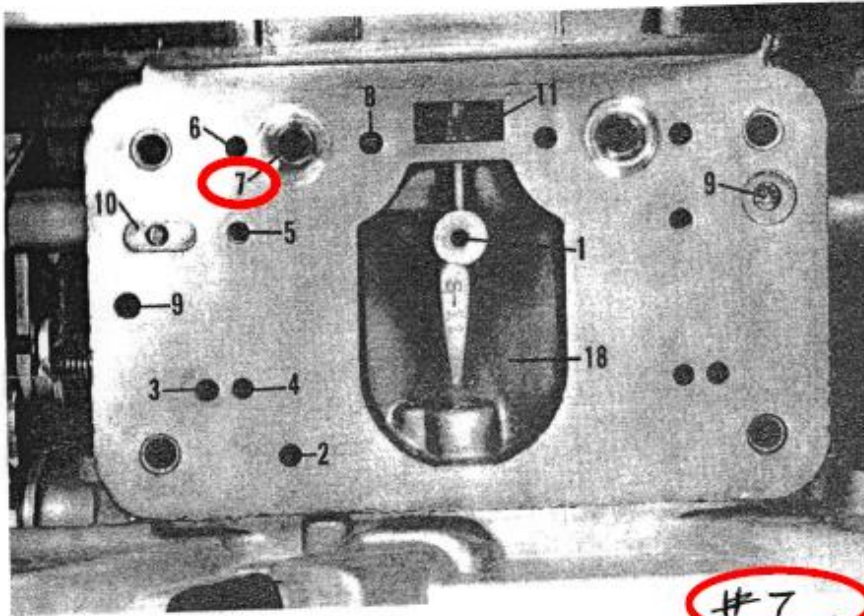


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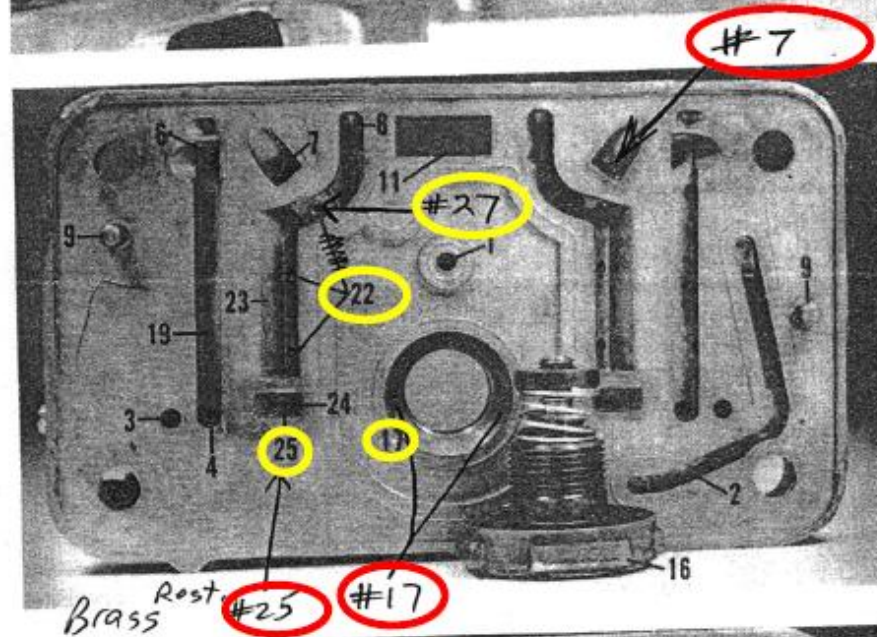
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Holley 4412 Metering Block Measurements

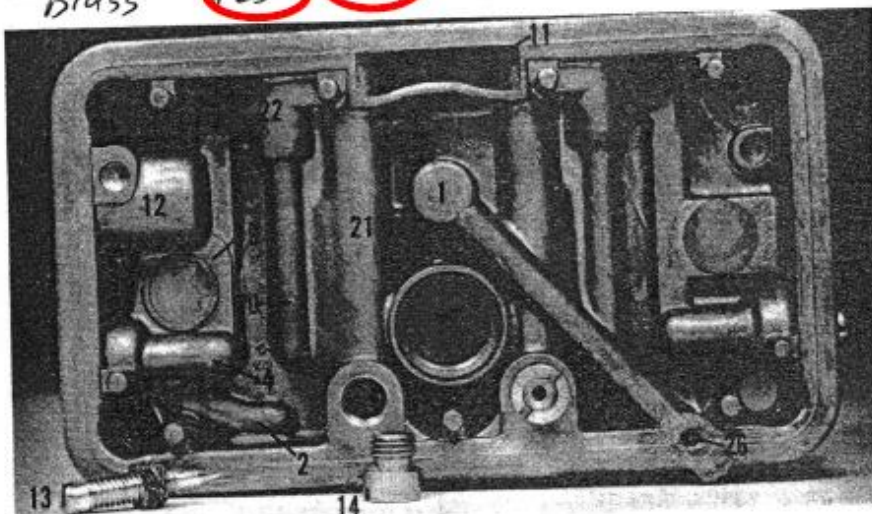


- Description of holes in main body gasket surface joining to metering block:
- 1 - To discharge nozzle (accel. pump)
 - 2 - To timed-spark port
 - 3 - To curb-idle discharge
 - 4 - To idle-transfer slot
 - 5 - Used only with auxiliary idle air bleed
 - 6 - To idle air bleed
 - 7 - To main discharge nozzle
 - 8 - To main air bleed
 - 9 - Dowel locators for metering block
 - 10 - Not used
 - 11 - To bowl vent (pitot tube)



Description of holes and passages in metering block. Top photo shows side which mates to main body gasket. Lower photo is bowl side of metering block.

- 1 - Accelerator pump discharge passage
- 2 - Timed-spark passage (see 12)
- 3 - Curb-idle discharge
- 4 - Idle-transfer fuel connects to main body and to curb idle adjust screw
- 6 - Idle bleed air enters from main body
- 7 - Main passage to discharge nozzle
- 8 - Main bleed air enters from main body
- 9 - Dowels to position block & gasket
- 11 - Bowl vent passage
- 12 - Timed-spark tube boss
- 13 - Idle-mixture-adjustment needle
- 14 - Main jet
- 15 - Power valve threaded opening
- 16 - Power valve
- 17 - Power valve channel restriction (connects to main well 21)
- 18 - Manifold vacuum chamber (for power valve operation)
- 19 - Idle down well
- 20 - Idle well
- 21 - Main well
- 22 - Air bleed holes into main well
- 23 - Main air well
- 24 - Idle fuel from main well
- 25 - Idle feed restriction to idle well
- 26 - Fuel entry from accelerator pump in fuel bowl
- 27 - Air Bleeds



Brighton Speedway Concerned Measurements
 Holes #7 - .141"
 Holes #17 - .063" - .065"
 Holes #22 - .027" - .029"
 Holes #25 - (Brass insert) .035"
 Holes #27 - .027 - .029



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THUNDER STOCK CLASS

General Rules, Race Procedures, Safety Procedures and Tear Down Procedures in the front of this book apply to all classes.

Starting in 2019 Brighton Speedway will follow the Ontario Dirt Competition Committee Thunder Stock Rules with the following notes and exceptions.

3.0 CHASSIS - Offset Chassis will be grandfathered in 2019 drivers were responsible to register their off-set chassis with officials and these were documented. No new Offset Chassis will be allowed after 2020

9.0 ENGINE and CARB – Brighton Speedway will continue to follow the Brighton Speedway Crate OR ODCC Crate sealing, and Open engine rules.

9.0 For Thunder Stock Engine Rules please see the Canadian Modified Engine rules with the following exceptions:

9.1 Stock Harmonic Balancer must be used.

9.2 Stock Style Pulley's must be used OR the following aftermarket pulleys: ALL31080, ALL31082, ALL31083, PCE425-1040, KRC36400000, KRC36367500 or KRC364030000 or other aftermarket pulley approved by the speedway.

9.3 Aftermarket power steering pumps allowed.

9.4 Stock Style cooling fans, no electric fans.

9.5 Pro Stock carb adapter / spacer must BRP 377 with no modifications.

9.6 Intake/breather heat shields or air boxes not allowed.

10.0 EXHAUST Full ODCC rules in effect

11.0 TRANSMISSIONS & CLUTCHES – ODCC Rules in Effect

13.0 TIRES & WHEELS – Brighton Speedway will remain on Hoosier Tires Only for weekly competition.

If there is something else in your car that does not meet the rules due to 2019 rule changes feel free to discuss with Speedway Officials to decide if they qualify for a "grandfather" clause.

The complete ODCC Thunder Stock rules can be found at
<https://ontariodirtcomp.com/>



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COMP 4 CLASS

General Rules, Race Procedures, Safety Procedures and Tear Down Procedures in the front of this book apply to all classes. *Note is 2025 Comp 4 and Mini Stock will run together in one division for one points championship, with exception of Dual on the Dirt Events.*

Comp 4 class racing is designed for the amateur individual participating in racing cars as a hobby. The rules and restrictions are designed to prevent the cost of competing from getting out of hand, and to give each individual a fair and competitive chance in a division, in which the enjoyment of winning and accomplishment is just as exciting as in the faster, more complicated divisions.

1.0 CAR MODEL

- 1.1 Any 4 cylinder car.
- 1.2 No convertibles, station wagons, minivans, mid or rear engine vehicles and no Mazda rotary engines.
- 1.3 No rear wheel steer cars allowed
- 1.4 **Cars built up until 2004 may be raced. Any car built 2005 and after must be pre-approved by Speedway Officials.**
- 1.5 No race purpose manufacture-built chassis or components, (i.e. Not type R Honda's)

2.0 BODY & BUMPERS

- 2.1 **Body must be presentable at all times. All panels must retain stock** (homemade doors allowed as long as they retain stock bodylines).
- 2.2 Any door or body skirts must remain flexible and not be a treat to competitors tires
- 2.3 Doors must be welded or securely fastened.
- 2.4 All inner steel panels must be retained except for roll cage clearance. Hood and trunk lids may be skinned on any rear wheel drive car. Any other car that wishes to skin hoods and trunks for weight saving must first check with track officials. All panels must be made of steel other than plastic nose and tail bumper covers.
- 2.5 Factory wheel arches must be retained.
- 2.6 Original grill openings must be retained and filled with mesh or screen.
- 2.7 Trunk type rear doors may be removed, but lower section must be filled in from bottom to the level where the lowest part of the window would be, only steel sheet metal of no less than 20 gauge to be used.
- 2.8 Spoiler on trunk lid or roof (for hatch backs) allowed but is not to exceed 4" high.
- 2.9 Front firewall must remain original with all holes filled with no less than 20 gauge steel.
- 2.10 Rear firewall and top shelf must be completely closed in no less than 20 gauge steel.

Body and Bumpers Cont.

- 2.11 Floors must remain stock, complete and in original position from firewall to rear trunk. Any holes to be patched only with 20 gauge metal
- 2.12 Fenders and splash pans to remain stock
- 2.13 All body patched, head light covers, filler panels ect. must be steel. Hatch back rear passenger windows can be filled in with aluminum or plexi-glass.
- 2.14 All metal work must be welded or riveted, no sheet metal screws anywhere.
- 2.15 Bumpers may be stock or rebuilt out of tubing underneath stock bumper caps or matching aftermarket plastic front and rear bumper covers. No DIRT or PAVEMENT Late Model nose cones. Aftermarket nose cones must be of the street stock style and not a modified or late model nose cone. If you have a question ask questions before buying a nose cone.
- 2.16 A protective bar may be installed on either side of the car with maximum of 1"x 2" not protruding more than 1" from the body. It must be mounted flush to the body no sharp edges with ends capped, and painted the same colour as the car. No other exterior bars allowed.
- 2.17 Tin kits are allowed behind the driver's seat but the stock floor and rear fire wall must remain in place.

3.0 CHASSIS, SUSPENSION

- 3.1 Chassis and suspension parts must remain stock with no modifications.
- 3.2 Camber kit may be used on front or rear. No camber rule is in place provided that the frame, sub-frame r wheel wells are not cut for excessive camber
- 3.3 Uni-body cars must be reinforced between front and rear sub frames.
- 3.4 Spring and shock mounts may be supported at the top.
- 3.5 Built or bent chassis to gain advantage shall be deemed illegal.
- 3.6 Strut support bars from side to side are allowed.
- 3.7 Front and Rear aftermarket adjustable upper / lower control arms are allowed however no excessive camber as per 3.2 and no rear steer allowed.
- 3.8 Any welding or modifying of suspension to straighten or bring the car back into tolerance must be pre-approved in writing by Speedway officials.

4.0 ENGINE

- 4.1 Engine must be mounted in stock location and position
- 4.2 Make and model must match chassis manufacturer and must fit into the car with no modification to the chassis or the engine mounts
- 4.3 No factory performance racing inspired engines. Example: no type R, no type S engines
- 4.3 No turbo chargers allowed.
- 4.4 No adjustable cam gears.
- 4.5 All casting numbers must remain unaltered and visible.
- 4.6 No decking of the engine block. No porting or polishing of any kind
- 4.7 Maximum of .030" overbore allowed.
- 4.8 Head resurfacing allowed. Cylinder head must retain 95% of combustion chamber after resurfacing.
- 4.9 Any hydraulic cam shaft that fits with no machining of cam or engine.
- 4.10 Mustangs are allowed any STOCK ford two barrel carburetor that can bolt on with out an adapter or modification to the intake or the carb **OR a Holly 500 (4412) Carb.** Note if running the Holley 500, the carb will be subject to passing speedway gauges.
- 4.11 Aftermarket air filters are allowed however stock air intake hoses and tubes must be retained. No ram air or cold air intakes allowed.
- 4.12 Transmissions must remain stock with all working gears, transmissions may be welded.
- 4.13 Flywheels must be stock for North American make and model, NO Aluminum, No Light Weight flight wheels

5.0 EXHAUST

- 5.1 Exhaust must be stock for make and model of car or Y-Pipe into single exhaust. If homemade Y-pipe is used it must be 2 into 1 before the exhaust bends under the engine.
- 5.2 **Must exit behind the driver**, preferably on the passenger's side and must be securely mounted.
- 5.3 Maximum exhaust size 2" outside diameter. **No Exceptions.** Exhaust must remain the same dimension from front to back, squaring or oval shaping the tip.
- 5.4 Mufflers are now mandatory for Comp 4 and are available from Brighton Automotive or the APC (Belleville) Auto Parts Bix Max Muffler . Mufflers have a 2 ¼ inlets so they will slide over existing exhaust. (Ask for the Brighton Speedway Comp 4 Muffler)
- 5.5 Muffler must be the last component of the exhaust and officials must be able to look up the back of the muffler for inspection purposes.

6.0 FUEL SYSTEM

6.1 Fuel Cells Mandatory and must be mounted securely in the trunk area with all steel straps, as far forward as possible between the frame rails. It must be separated from the driver's compartment by an all-metal firewall. Fuel cells may be lowered below the floor but must still be fully enclosed in a steel box. This box must not be lower than the centerline of the rear end.

6.2 No less than 20 gauge steel.

6.3 Fuel lines to be mounted in a safe manner and are subject to the approval of speedway officials. If run through the cockpit, lines must be steel lines (no rubber) and shielded where they pass through fire walls.

6.4 Cars originally equipped with an electric fuel pump **must have** an oil pressure switch functional to shut off fuel supply in case of a rollover.

6.5 **NO POOR INSTALLATIONS**

7.0 TRANSMISSIONS & CLUTCHES

7.1 Stock only for year, make and model.

8.0 REAR END

8.1 Stock only for year, make and model.

8.2 Rear ends may be locked by welding only, Same for FWD cars.

9.0 BRAKES

9.1 Stock only.

9.2 Emergency brakes and hand brakes must be deactivated (handles must be removed), only normal pedal hydraulic brakes that activate all four wheels equally and at the same time are allowed.

10.0 TIRES & WHEELS

10.1 Wheels must be production steel reinforced passenger type rims maximum of 7" width.

10.2 Wheels may be of offset. No cutting and creating homemade offsets.

10.3 Minimum 1" Wheel nuts required on both R/H side wheels

10.4 Maximum 1/2" wheel spacers may be used providing larger wheel studs and lug nuts are used.

10.5 Tires must be D.O.T. street tires with maximum 7" tread face. All tires subject to being measured by Brighton Speedway tire width bar.

10.6 No snow, winter or ice tires or tire cutting or sipping allowed. Light grinding is allowed.

10.7 No Competition / racing tires of any sort allowed even if the tire has a DOT stamp.

11.0 WEIGHT

11.1 **2025 Weights will be 2500 for non-V-Tec and 2650 for V-Tec**

11.3 Structural integrity must be retained while trying to meet minimum weight.

11.4 The weight of Brighton Speedway Scales will be official.

11.5 All added weight must be securely bolted to a main member of the chassis or roll cage and must be painted white with the cars number on it.

Special Rules

- *In the interest of competition, The Speedway reserves the right to adjust weight rules through out the season as it sees fit.*
- Anything not being specified as being allowed must be of stock origin and application.

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2023-2026 Mini Stock Official Car Specifications

These rules will be locked in for three years however annually there may be amendments that the track owners or the technical team feels are needed as a clarification or a result in technology.

Note is 2025 Comp 4 and Mini Stock will run together in one division for one points championship, with exception of Dual on the Dirt Events.

All questions and comments regarding rules should be addressed to the Ontario Dirt Competition Committee web site; <https://ontariodirtcomp.com/contact/>

TERMS:

The term "Stock O.E.M. replacement" means that the item/part must have been a standard item/part on the MAKE and MODEL of the vehicle being driven as a racecar. Just because the company that manufactured the vehicle manufactures a part does NOT mean the part is legal. The part must have been available on the specific vehicle being used as a racecar. Stock O.E.M. replacement direct fit, direct replacement part only. Must meet size/weight/material requirements as if factory produced. No Modifications.

The term factory stock/ production or factory produced:

Manufactured by the company who produced the vehicle being used as a race car. Manufactured for the year/make/model of the racecar

REMEMBER – BUILD IT LEGAL, KEEP IT LEGAL AND HAVE FUN!

In the spirit of equalizing competition, adjustments may be made as becomes necessary.

1.0 – Mini Stock Competing Models

- 1.1 – Open to all cars manufactured between 1979 and current year, self-aspirated or fuel injected, no turbo or supercharged automobiles.
- 1.2 – No convertibles, station wagons, jeeps, trucks, etc.
- 1.3 – Front or rear wheel drives allowed, no all-wheel drive or four wheel drive automobiles allowed. No rear or mid-engine cars.
- 1.4 – No Porsche.
- 1.5 – CRX and any 2 passenger cars will not be allowed.

2.0 – Engines

- 2.1 - Three (3) or Four (4) cylinder engines only, must be stock for make, model and year.
- 2.2 – ODCC Committee has the right to make a weight rule to equal out the competition.
- 2.3 - Engines no larger than 2.5 liter (2500 cc.)
- 2.4 – 16 valve engines allowed single cams only.
- 2.5 - No V-Tec
- 2.6 – The following makes and models of dual cam 4cylinder equipped cars will be permitted at a weight of 18.5lbs/Factory Rated Horsepower +100 lbs.:
 - -1997- 2011 Ford Focus/ZX2
 - -1999- 2002 Mercury Cougar
 - -1994- 1999 Chrysler Neon
 - -1995- 2005 Cavalier/Sunfire
 - - 2005- 2010 Cobalt/Pursuit/G5 (Any 2.2L) No F35 SS Transmissions

- [- 2003 - 2008 Mazda 3 2.0 L only \(no 2.3 L\)](#)
- -1991- 2005 Saturn Ion
- -1996- 2005 Grand Am/Alero
- 2.7 - No Rotary type engine.
- 2.8 - 2 spark plugs per cylinder allowed.
- 2.9 - No performance or truck parts of any kind allowed.
- 2.10 - No engine modifications to enhance performance are allowed.
- 2.11 - No porting or polishing allowed.
- 2.12 - Valve size must be stock for that engine.
- 2.13 - Milling of head allowed. Must not exceed 190 psi compression.
- 2.13.1 - **Engines 200 psi – 210 psi will add 100 lbs. minimum weight. Cannot exceed 210 psi. 211psi and over cannot compete. No exceptions.**
- 2.14 - Maximum 0.040 overbore.
- 2.15.1 - 8 valve engine may run any lift, any duration camshaft.
 - 2.15.2 – 12 valve engines may run 0.030” over stock valve lift with 50lbs added to minimum weight.
 - 2.15.3 – Camshaft Lift must be Canadian or U.S. specs.
 - 2.15.4 – Valve springs can be higher pressure than stock.
 - 2.15.5 – Multi index timing gears or adjustable sprockets are allowed on 8 and 12 valve engines only.
- 2.16 – 16 valve engines must have stock or stock replacement camshaft only. Lift and duration must be stock. Valve springs must be stock.
- 2.17 - No after-market or performance ignitions allowed. Must remain stock.
- 2.18 - No mass air modifications. Must remain stock.
- 2.19 - No intake modifications. Must remain stock.
- 2.20 - Engine size must be placed on the hood in CC or CID.
- 2.21 - Must remain stock stroke.
- 2.22 - Stock connecting rods and rod caps. No grinding or polishing.
- 2.23 Engine and any engine parts must be for Canadian and US models.

3.0 – Carburetor

- 3.1 - Carburetor must be an OEM type. Maximum stock 2 barrel with no alterations. Or Holley (Part #7448) 350 cfm 2 barrel carburetor. Choke can be removed and jets can be drilled larger or changed. No other alterations.
- 3.2 – Any 1” adapter or spacer. Intake can be cut to match butterflies of Holley Carburetor.
- 3.3 - Electric fuel pumps allowed with inertia or oil pressure switch.
- 3.4 - Any air filters allowed. K&N type air filters allowed.
- 3.5 - Air breather lid and bottom must be metal. Can be trimmed to the outside diameter of the filter.

4.0 – Fuel Injection

- 4.1 - Fuel injection and electric fuel pumps are allowed if equipped as stock.
- 4.2 - A matched intake and carburetor set-up may replace fuel injection set-up with no modifications.
- 4.3 - Electric fuel pump must have inertia or oil pressure shut off switch to shut off fuel pressure in the event of an accident.
- 4.4 - Any air filters allowed. K&N type air filters allowed.
- 4.5 - Air breather top or bottom can be trimmed air must be pulled through stock size filter.
- 4.6 - Stock tube between air filter and throttle body may be replaced but must remain stock diameter and length.
- 4.7 - Computer must be stock with no modifications, no removing of rev. limiter.
ECM: Stock ECM only no modifying or reprogramming. ECM can be claimed by the track for \$200.00. If ECM is modified, reprogrammed or claim refused car/driver will be disqualified and loss of all points to date. Driver will be suspended for one year. ECM can be exchanged by official at any time.
- 4.8 – OBD II equipped cars must always have a functioning diagnostics port accessible to tech.

5.0 – Fuel System

- 5.1 - Fuel Cells MANDATORY and securely fastened in the trunk located and vented. No sloppy installations.

- 5.2 - Fuel cell filler neck must be grounded to the car.
- 5.3 - Fill spout to be located inside trunk.
- 5.4 - No aluminum tanks.
- 5.5 - Full firewall required between driver's compartment and trunk area.
 - 5.6 - Automotive Pump Gas only. No race fuels. No additives.
 - 5.7 – Plastic fuel cells must be encased in a steel box. (Recommended 20-gauge steel)

6.0 – Cooling System

- 6.1 - Aluminum radiators with plastic tanks allowed for any year make model and style of car from manufacturer only in stock location.
- 6.2 - Aftermarket radiators or racing radiators are allowed.
- 6.3 – Oil coolers allowed.

7.0 – Electrical System

- 7.1 - Battery must be mounted securely in a Spill proof Container in trunk, under hood, or behind driver seat.
- 7.2 - It is recommended the battery be mounted under the hood and be securely fastened.

8.0 – Exhaust

- 8.1 – Stock cast iron exhaust manifold or factory header allowed. Aftermarket header allowed with 50 lbs added to minimum weight.
- 8.2 - Exhaust must exit behind driver's door securely fastened.
- 8.3 - No part of the exhaust allowed in the driver compartment.
- 8.4 - Maximum 2 1/2" OD. exhaust pipe. 2" OD maximum exhaust pipe on cars with engine over 125 hp. By manufacturer's specs. 1 3/4" OD. maximum exhaust pipe on cars equipped with variable valve timing. Entire exhaust system must be specified size behind the downpipe and Minimum 36" long.
- 8.5 - Stock or stock replacement mufflers are allowed.
- 8.6 - Exhaust must be securely fastened.

9.0 – BODIES

- 9.1 - Complete stock from original design of make, model and year. Altered bodylines will be significant reason for disqualification. No hood scoops. 5" rear spoiler is allowed.
- 9.2 - Floorboard covering such as carpeting, door panels, headliner, and insulation must be removed.
- 9.3 - All holes in floor and firewall must be filled, patched with 18 gauge sheet metal.
- 9.4 - Rear seat must be removed.
- 9.5 - Hood, roof, doors and trunk lid may be gutted. Other inner panels may be gutted as long as the driver is protected.
- 9.6 - All doors must be welded or bolted shut.
- 9.7 - Side scrub rails allowed and must be rectangular tube (1" x 2" ends beveled tight to body)
- 9.8 - Frame connectors are allowed.

10.0 – Appearance/Numbering

- 10.1 - All cars must be neatly and brightly painted.
- 10.2 - Numbers as well as letters should be as large as possible (opposite colour of background) on doors, roof and all four corners of the body. (roof number must be read from right hand side of the car)
- 10.3 - Sponsor decals must be placed on designated location.
- 10.4 – Year, make, model, engine size in CC, and declared weight must be on hood.

11.0 – Bumpers

- 11.1 - Bumpers must be securely mounted. Fabricated bumpers allowed but must have a stock appearing cover and must be approved by official.
- 11.2 - Tow hook must be installed one on the front and one on the rear of the car for towing purposes.

12.0 – Glass

- 12.1 - All glass must be removed.
- 12.2 - Windshields must be replaced with Lexan or full wire screen of 1” square maximum.
- 12.3 - All external lights must be removed.
- 12.4 - No mirrors.

13.0 – Firewall

- 13.1 – Car must retain stock front steel firewall in original location and must be sealed to the hood. All holes patched. (20 gauge minimum)

14.0 – Frames

- 14.1- Stock frames only. No modifications allowed except for re-enforcement.
- 14.2 – Frame stock appearance must be maintained when strengthening is required so as not to reconstruct as a tube frame.

15.0 – Radios and Communications

- 15.1 - No two way radios are allowed.
- 15.2 – A working one way radio is mandatory.
- 15.3 - Frequency is 454.000.
- 15.4 - RACECEIVER One way radios are recommended.
- 15.5 – No Mobile Devices allowed in cars during competition.

16.0– Suspension and Transponders

- 16.1 – Suspension
- 16.1.1 - Stock OEM replacement parts only. No modifications allowed.
- 16.1.2 - Aftermarket bushings and bearings allowed. Must be a direct replacement for stock bushings. FWD cars may replace rear wheel bearings with upgraded bearings. Upgraded or aftermarket bushings and bearings must not alter stock suspension geometry.
- 16.1.3 - No racing shocks.
- 16.1.4 - Springs can be any size or spring rate. Do not have to be the same rate side to side. Springs can be cut. Spring spacers allowed.
- 16.1.5 - Front strut mount holes may be elongated to achieve front camber. Other methods must be safe and approved by the Tech Director.
- 16.1.6 - Front and rear lower control arms must be stock length
- 16.1.7 - Rear camber allowed within stock adjustments. No cambered rear ends (bending axle tubes).
- 16.2 – Transponders

- 16.2.1 – Transponder must be located 18” back from center line of rear end or rear hub to front leading edge of the transponder or further back is OK. Please also keep as close to the lower part of the frame as possible. DO NOT MOUNT HIGH
- 16.2.2 - Cars with Transponders found in locations other than that indicated above during post-race inspection will be penalized.

17.0 – Steering

- 17.1 - Stock steering for the make and model. No modifications allowed.
- 17.2 - No steering quickener allowed.
- 17.3 - Steering wheel quick release allowed.
- 17.4 - Center must be padded.
- 17.5 - Must have collapsible steering column.

18.0 – Rear ends, Transmission, Clutches, & Flywheels

- 18.1 - Any stock transmission for make, model. No modifications allowed.
 - [No GM F35 transmissions allowed.](#)
- 18.2 - All gears must be in working order.
- 18.3 - Must have two (2) safety hoops 2” x ¼” within 6” of either end or drive shaft.
- 18.4 - Driveshaft must be painted white.
- 18.5 - Locking of rear ends or differentials allowed.
 - [RWD cars may change rear end gears using stock OEM parts.](#)
- 18.6 - Stock unaltered rear end for that make, model and year and must remain square in car.
- 18.7 - Clutch pedal must remain stock for make, model and year.

19.0 – Wheels, Tires, & Brakes

- 19.1 – Right side must have Steel wheels only. No less than 3” backspacing allowed.
- 19.2 - Aluminum wheels allowed on left side only.
- 19.3 - No mag wheels or wire spoke wheels.
- 19.4 – Racing rims allowed and recommended.
- 19.5 – Wheels must not stick out past the rub rails or body.
- 19.6 - Maximum width of seven inches. (measured inside of bead to inside of bead)
- 19.7 - Must use 1” lug nuts. ½” stud recommended.
- 19.8 - Wheel spacers are not permitted.
- 19.9 – DOT Radial Tires only. No mud, no snow, no ice, no light truck, no all-terrain tires, no racing inspired or designed tires of any sort (even if DOT approved). All season or four season tires allowed. No siping, grooving, cutting or modification the tires allowed. Light grinding is allowed. Tires must have a UTQG tread ware rating 200 or higher.
- 19.10 - Stock brake system for make, model and year. All brakes must function at all times. No adjustable brake bias. No after-market proportioning valves.
- 19.11 - Brake pedal must remain stock for make, model and year.
- 19.12 – All Emergency Brake Handles and Pedals must be disconnected and [completely removed from the car during race events. If a car pulls over the scales or through tech with the brake handle installed it will be automatically disqualified.](#)

20.0 – ROLL BARS

- 20.1 - A minimum six-point roll cage is required. Minimum 1 ½ diameter OD. Minimum thickness .120” wall or 1-3/4” x .095 is recommended.
- 20.2 - Must have a minimum of four (4) side door bars on the driver’s side running between the front and rear upright bracing. A minimum of two on the passenger side.
- 20.3 - Front and rear hoop allowed in front of rad and behind fuel cell and must be concealed behind sheet metal and stock bumpers.
- 20.4 - Galvanized, aluminum, or exhaust type tubing is not permitted.

- 20.5 - Approved padding must be used at any point where driver contact might occur.
- 20.5 - Original frame profile must be maintained while constructing the roll cage.
- 20.6 - Intrusion plates are highly recommended on the driver's door roll bars. Intrusion plates may be welded in the door bars or clamped on the exterior door bars. Intrusion Plates as per pictures below. These are becoming mandated by many sanctioning bodies as they are widely accepted as an important part of driver safety and may become mandatory in the future.



21.0 – Weights

- 21.1 – Cars with 8 valve engines must weigh 1 lb/cc. Must be 2000lbs minimum.
- 21.2 – Cars with 12 valve engines must weigh 1.1 lbs/cc. Must be minimum 2000lbs.
- 21.3 – Cars with 16 valve engines must weigh a minimum of 18.5 lbs/Factory Rated Horsepower must be a minimum of 2300lbs.
- 21.4 – All Volkswagens Must weight 2500 lbs minimum ALL IN!
- The only weight break/penalty for Volkswagens will be rule 21.9.1 (head and neck restraint -25lb Break)
- 21.5.1 - Maximum left side weight 55% with driver's belts tight. Removal of mud will be at the discretion of the Technical Inspector.
- 21.5.2 – Added weight must be securely fastened with a minimum of two half inch bolts, weight must be painted white with car number. No floor board or body metal mounting of weight.
- 21.6 – Added weight in the driver's compartment must be approved by the Technical Inspector.
- 21.7.1 – 100 lb added to minimum weight for approved DOHC engines
- 21.7.2 – 50 lbs added to minimum weight for 0.030" more than stock lift cam on 12 valve engines.
- 21.8 – 50 lbs added to minimum weight for aftermarket header.
- 21.9.1 – 25 lbs weight break from minimum weight if a head and neck restraint meeting SFI 38.1 or greater is worn.
- 21.9.2 – 200 lb weight break for automatic transmission.



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Pro Late Models

Late Models will be NLMA Sanctioned in 202~~53~~ and the rules will be posted shortly on the all new NLMA web site. Brighton Speedway and the NLMA will continue to have an open fuel rule but reminds drivers that choose to run race fuel that VP Race Fuels supports the NLMA, track and the driver's points fund significantly

2025 NLMA Late Model Rules can be found at <http://nlmaracing.com> . In addition to the NLMA Rules Brighton Speedway requires the following additions:

- 1.0 A cockpit screen **with maximum 1" sq holes** and vertical support bars minimum 1/4" thick, no more than 6" apart in the windshield opening
- 1.1 Tow cable or hook to be mounted on the front nose of the car to make easy access for towing.
- 1.2 Rear bumpers must be smooth with no sharp or pointed corners that will cut nose cones (i.e. Pierce Race Cars jacking posts).
- 1.3 Aluminium or Fiberglass wheel covers must be bolted on, no ZUS fasteners allowed. Foam wheel covers are preferred.
- 1.4 **Holley 26-342 Fuel Bowl Vent Tube; Spill Reduction Valves recommended.**
- 1.5 No lithium batteries allowed.
- 1.6 Crate USA or Rush D55 Tires only on all four corners.
- 1.7 NLMA and Brighton Speedway will allow Brighton Speedway seals in both 602 and 604 motors in 2023 and beyond. Lukes Machine Shop and Workman Auto Repair are authorized to seal 604 motors. This will allow drivers to save the expense of RUSH seals if they have no interest in running RUSH events and only running BSP or NLMA Events.

Brighton Speedway rules that will still take precedence are the majority of the General Rules, the majority of the Race Procedure Rules, the majority of the Car and Driver Registration Rules and Flagging Procedures



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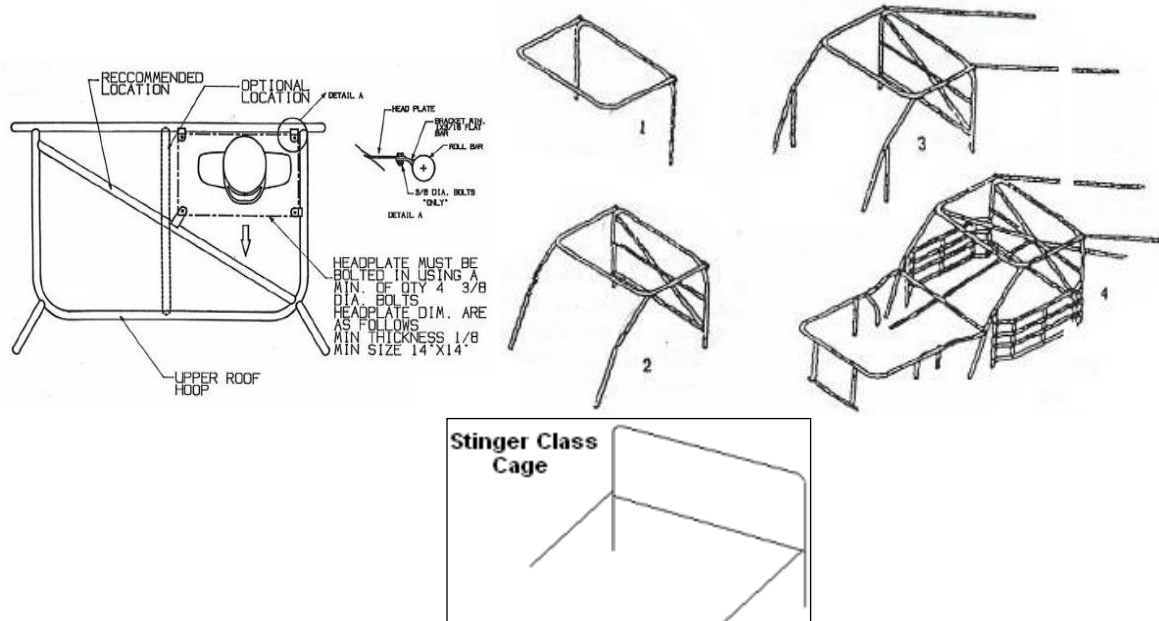
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Six Cylinder Stingers Rules

1.0 Concept

- 1.1 6 cylinder front wheel drive cars, only North American original builders (GM, Ford or Chrysler) products. Cars must remain stock and unaltered for performance other than the stripping of interior and exterior for racing purposes.
- 1.2 Limited to 8 – 10 races per year
- 1.3 No Purse, trophies only
- 1.4 Points will be kept and awards given out
- 1.5 Claimer rule will be in effect to keep car costs in line
- 1.6 If drivers show up to consecutive events they receive free admittance to event THIS BONUS IS FOR THE DRIVERS ONLY AND NOT THE CAR, THE DRIVER MUST BE COMPETING IN CONSECUTIVE EVENTS
- 1.7 These cars will be eligible to race in BSP Enduro events throughout the year if desired.
- 1.8 This is a ZERO tolerance non-contact class. Any driver making contact with another car will be black flagged. If the same driver receives a second black flag they will receive a 1 week suspension. Further black flags will receive additional suspensions.
- 1.9 JR Stinger is considered a driver with 2 years or less of racing; Super Stinger is a driver in his 3rd year or more. Officials have the right to hold a diver in JR Stingers as they see fit. Final division split will be at the discretion of the officials.

2.0 Rules

- 2.1 1-way Radios are mandatory; see race procedure on page 5 of this rule book.
- 2.2 Any 6 cylinder FWD CAR, no turbo or super charged cars allowed.
- 2.3 Minimum of 1 roll bar and 1 driver's door and 1 passenger door bar required. Tubing must be 1 3/4" diameter and .095 thickness. (see diagram page 30)
- 2.4 Aluminum racing seat required
- 2.5 5 point racing harness required
- 2.6 Window net is required
- 2.7 Fire extinguisher optional
- 2.8 Body must be presentable at all times. Must have full body and all panels must retain stock
- 2.9 Doors must be securely fastened with 1/4" chain and 3/8" bolts.
- 2.10 All inner steel panels must be retained except for roll cage clearance.
- 2.11 Factory wheel arches must be retained.
- 2.12 Windshield must have steel screening with max 1" square holes.
- 2.13 Original grill openings must be retained and filled with mesh or screen.
- 2.14 Front firewall must remain original with all holes filled with no less than 20 gauge steel.
- 2.15 Rear firewall and top shelf must be completely closed in if fuel tank is in the trunk with no less than 20 gauge steel.
- 2.16 Floors must remain stock, complete and in original position from firewall to rear trunk. Any holes to be patched only with 20 gauge metal.
- 2.17 Plastic Inner Fenders splash pans can be removed.
- 2.18 All metal work must be welded or riveted, no sheet metal screws anywhere.

- 2.19 All glass, plastic, interior and exterior flammable materials must be removed. Original dash may remain
- 2.20 No outside re-enforcement allowed.
- 2.21 Gas tank must either be in the trunk strapped with at least 2" strapping with a firewall between driver compartment and trunk OR if the fuel tank is under the rear seat in front of the rear axle it may remain provided the straps are in good shape and the tank DOES NOT LEAK.
- 2.22 Bumpers must be chained to a solid body member.
- 2.23 Quick release hood and trunk (if Fuel tanks is in the trunk)with pins or seat belts (no nuts & bolts, keys or latches)
- 2.24 Serial or ID number on the top of the dash of the car cannot be removed or tampered with.
- 2.25 No Aluminium Wheels on the R/H side of the car, steel stock wheels only,
- 2.26 No snow tires (no snowflakes), no cutting, no grooving, no siping.
- 2.27 E-Brake handles/Peddles must be completely removed.
- 2.28 Diagnostic plugs must be installed and functional.
- 2.29 Feature winners will be required to weigh in every week.

3.0 Claimer Rules

- 3.1 Any car racing is eligible to be claimed up to 15 minutes after the main event of a particular race night
- 3.2 Car may be claimed by anyone included but not limited to: Competing Drivers, Pit Crew, The Track or Fans in the stands
- 3.3 Any person can only make maximum 2 claims throughout the season.
- 3.4 Any person refusing a claim will be disqualified from competing in future Stinger events or Speedway Enduro's
- 3.5 Complete car may be claimed for \$950.00 with the following exceptions are not included in the price:
 - Aluminium Seat
 - Racing Seat Belts
 - Window Net
 - Fire Extinguisher (if installed)
 - Battery
- 3.6 Driver being claimed is responsible to remove his/hers own equipment prior to the car changing hands. A BSP official must be present.
- 3.7 Person making the claim MUST be the driver of the car he/she claimed for at least 1 week after making the claim. If the car returns with a different driver other than the person that made the original claim an additional \$50.00 fine will be charged to the new driver prior to him/her being allowed to race.
- 3.8 No person may make a claim on a car if they live outside these boundaries without pre approval from Speedway officials: east than Nappanee, west than Oshawa, north of Hwy 7.
- 3.9 No Car may be claimed on or after Labour Day Weekend.

Special Rules

- ***In the interest of competition, weight penalties of 25 lbs will be added to cars after drivers first two feature wins. An additional 25 lbs will be added for every feature win after that. Weight penalties will remain in the car for the remainder of the season. Speedway Officials will determine where the added weight will be placed***

Any Questions regarding any rules in this book please contact the Brighton Speedway at (613) 475-1102 or mark@brightonspeedway.com .

Notes:

MONEY & POINTS SYSTEM

Points to apply to qualifying heats, features and any other event specified by the speedway. All earnings will be paid the following race meet to the driver only. Winnings must be redeemed within 30 days of the race meet. Points and money will be based on a guaranteed order of finish for all feature events.

Bonus Points will be award to all racers that participate in Speedway designated invitationals. Speedway officials will determine definition of participation and bonus points will be in the amount of 100 for your participation.

Late Models	
Pos	Regular Event
1	\$1,000.00
2	\$600.00
3	\$550.00
4	\$450.00
5	\$375.00
6	\$300.00
7	\$240.00
8	\$220.00
9	\$200.00
Back	\$200.00

Canadian Modified	
Pos	Regular Event
1	\$500.00
2	\$300.00
3	\$225.00
4	\$200.00
5	\$180.00
6	\$160.00
7	\$140.00
8	\$120.00
9	\$110.00
Back	\$100.00

Pro Stock	
Pos	Regular Event
1	\$350.00
2	\$250.00
3	\$175.00
4	\$130.00
5	\$105.00
6	\$95.00
7	\$85.00
8	\$75.00
9	\$70.00
Back	\$70.00

Comp 4/Mini Stock	
Pos	Regular Event
1	\$100.00
2	\$75.00
3	\$70.00
4	\$65.00
5	\$60.00
6	\$55.00
7	\$50.00
8	\$40.00
9	\$40.00
Back	\$40.00

Points (Points System Subject to Change)

Position	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24
Feature Points	150	140	130	120	110	100	95	90	85	80	75	70	65	60	55	50	45	40	35	30	25	20	15	10
Qualifier Points	50	45	40	35	30	25	20	15	10	5	And back													

INSPECTION SHEETS Can Mod, Late Model, Pro Stock, Comp 4, Mini Stock, Stingers

This is a list of what our Tech Inspector will be checking off in order for you to pass inspection. Safety stickers will not be given nor will cars be allowed on the track if anything on the list is not done, no matter how minor.

1. Helmets, Snell SA2015 or higher including Stingers
2. Approved fire suit (absolutely no holes or torn threads), Gloves and Neck Foam or Restraint
3. Kill switch, yellow triangle
4. Fire extinguisher in good charge and quick release holder or suppression system in good charge
5. Battery in battery case, battery cables through rubber sleeves in firewall
6. Fuel lines through rubber sleeves in trunk floor If applicable
7. Fuel line & Brake lines protected as required, **Fuel lines in the cockpit must be shielded and are at the discretion of the tech officials**
8. Fuel vent line must loop 3" above tank and exit through floor or new style tank with check valve
9. Gas tank and fuel cell mounted secure (with 20 gauge firewall for Pro and Comp 4)
10. Exhaust exiting behind driver and clear of any fuel or brake lines
11. 1/8" Scatter shield or steel bell housing for all cars with manual transmissions
12. Window net, anchored at bottom (optional in LM and CM with full containment seat)
13. Windshield vertical support bars, and 1" screen
14. Drive shaft painted white
15. Drive shaft safety hoop adequate
16. Brakes working on all four wheel for Pro, Comp and Stinger. LM and Mod may have RF shut off.
17. Parking brake handle/peddle removed (Comp 4 and Stingers)
18. 1" Wheel Nuts on all four corners (except Stingers)
19. Comp 4 if running off set wheels must use large "racing" wheel studs
20. Stingers no Aluminum Wheels on R/H side.
21. Hood pins only in hood and trunk (may still hinge if preferred)
22. Tow chains or cables mandatory on the front and back of Stingers, C4, Pro, and on the front nose of LM
23. Seat belts minimum 5 point mounted with no less than 7/16" bolts. Must not be with in the expiry date stamped on the belts.
24. Roll cage and gussets at all corners
25. Driver seat aluminum. Mandatory in all classes, must be mounted to the roll cage.
26. Body appearance in acceptable condition
27. One Way Receiver radios mandatory for all classes. AMB Transponders and mounting position (except Stingers)
28. Any aluminum or fiberglass wheel covers must be bolted on, no dzus fasteners. Foam wheel covers recommended.
29. Fuel Cell Roll Over Check Valve and Fuel Cell Vented Cap Flapper
30. Trailer / Pit Box Fire Extinguisher.